

STATE OF MAINE

STATE HIGHWAY COMMISSION



INTERSTATE 95

OVER

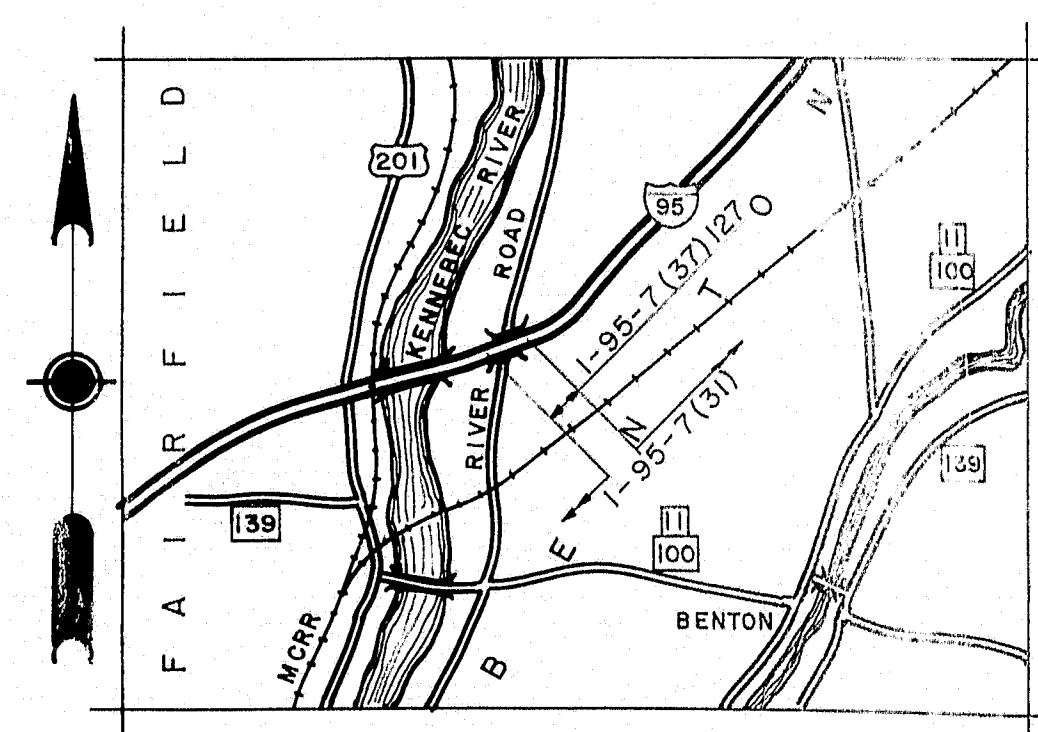
RIVER ROAD

IN THE TOWN OF

BENTON

KENNEBEC COUNTY

FEDERAL AID PROJECT NO. I-95-7(37)127
LENGTH OF PROJECT 0.074 MILES



LOCATION MAP
APPROX. SCALE - 1" = 1 MILE

INDEX OF SHEETS

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 10. GENERAL PLAN & QUANTITIES
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 19. SLOPE PAVING DETAILS, N.B. & S.B.
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 - 24, 25. SUPERSTRUCTURE, S.B.
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 28. REINFORCING STEEL SCHEDULE, S.B.
 29. REINFORCING STEEL SCHEDULE, N.B.
- STANDARDS - BD 101-62, BEARING PEDESTALS
BD 102-62, BRIDGE RAIL
BD 103-62, BEAM SPLICES

SCALES

PLAN & PROFILE { HOR. 1" = 20'
VERT. 1" = 10'
CROSS SECTIONS 1" = 10'

TRAFFIC

INTERSTATE 95	RIVER ROAD
6535	A.D.T. 1962 400
8880	A.D.T. 1982 560
1066	D.H.V. 56
11%	T 8%
60%	D 60%
60 MPH	V

APPROVED
MAINE STATE HIGHWAY COMMISSION

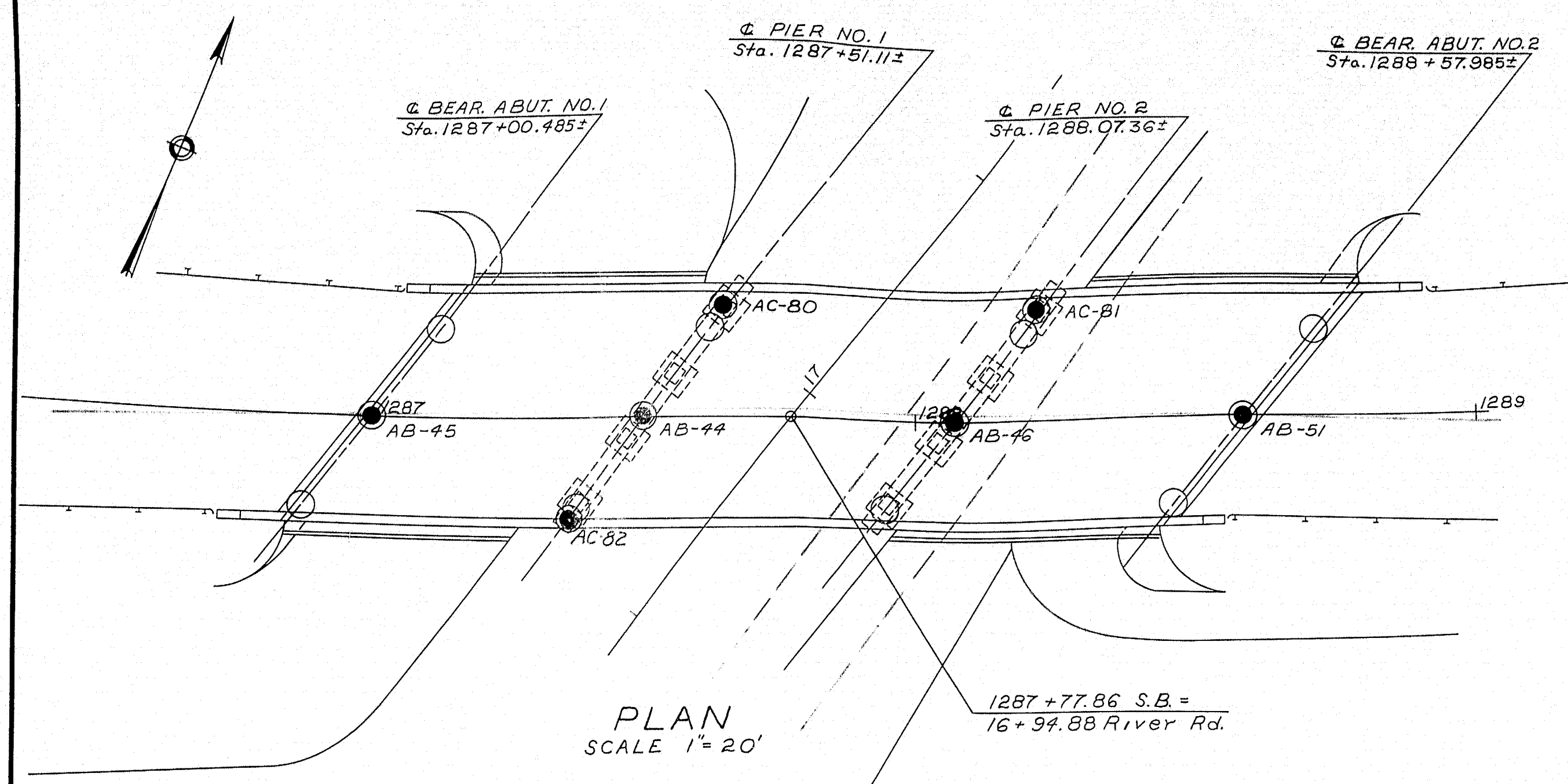
David H. Hines
CHAIRMAN
Edward J. Hines
CHIEF ENGINEER
4-10-63
DATE

DEPARTMENT OF COMMERCE
BUREAU OF PUBLIC ROADS
REGION 1

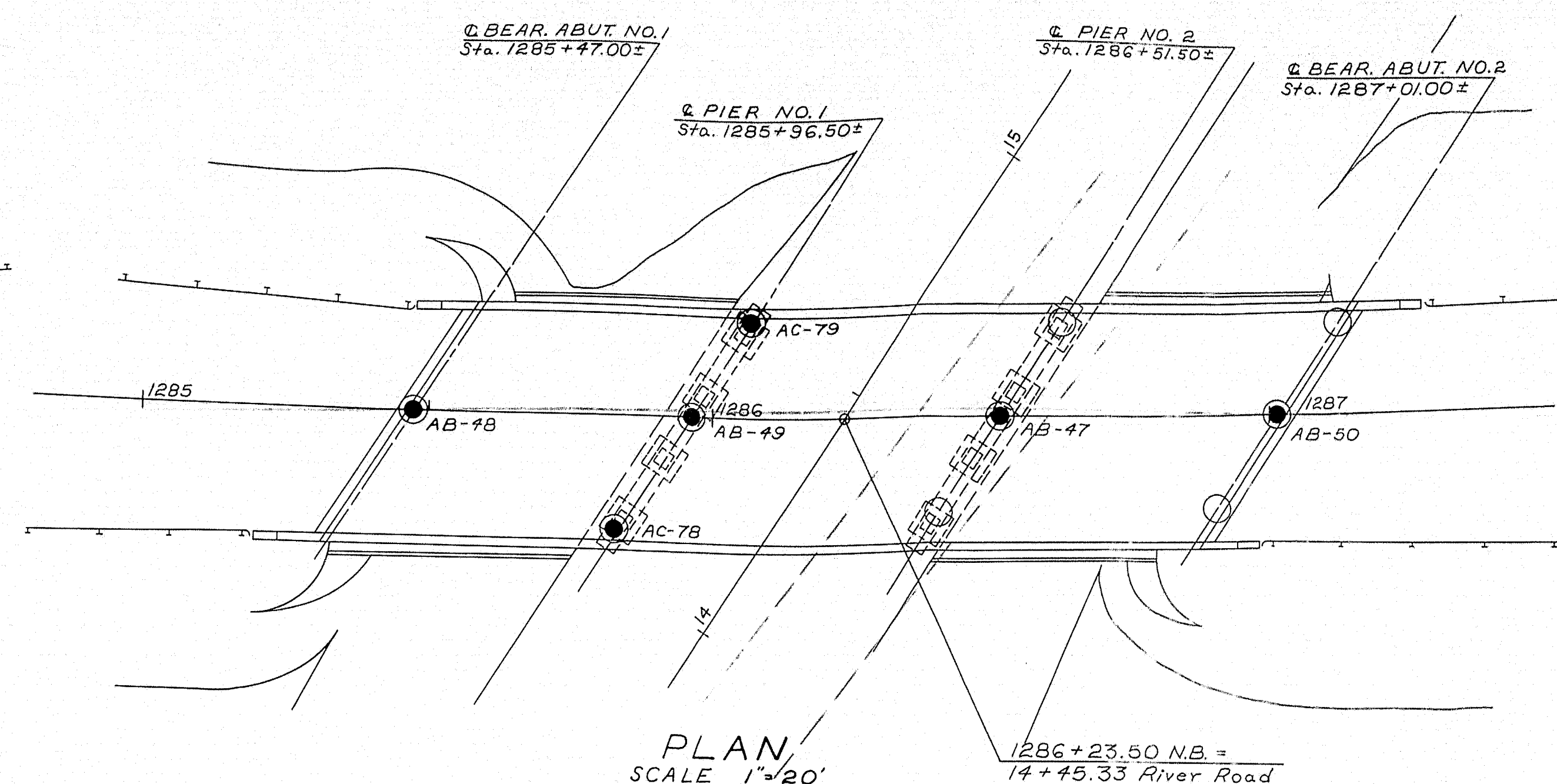
APPROVED

DIVISION ENGINEER DATE

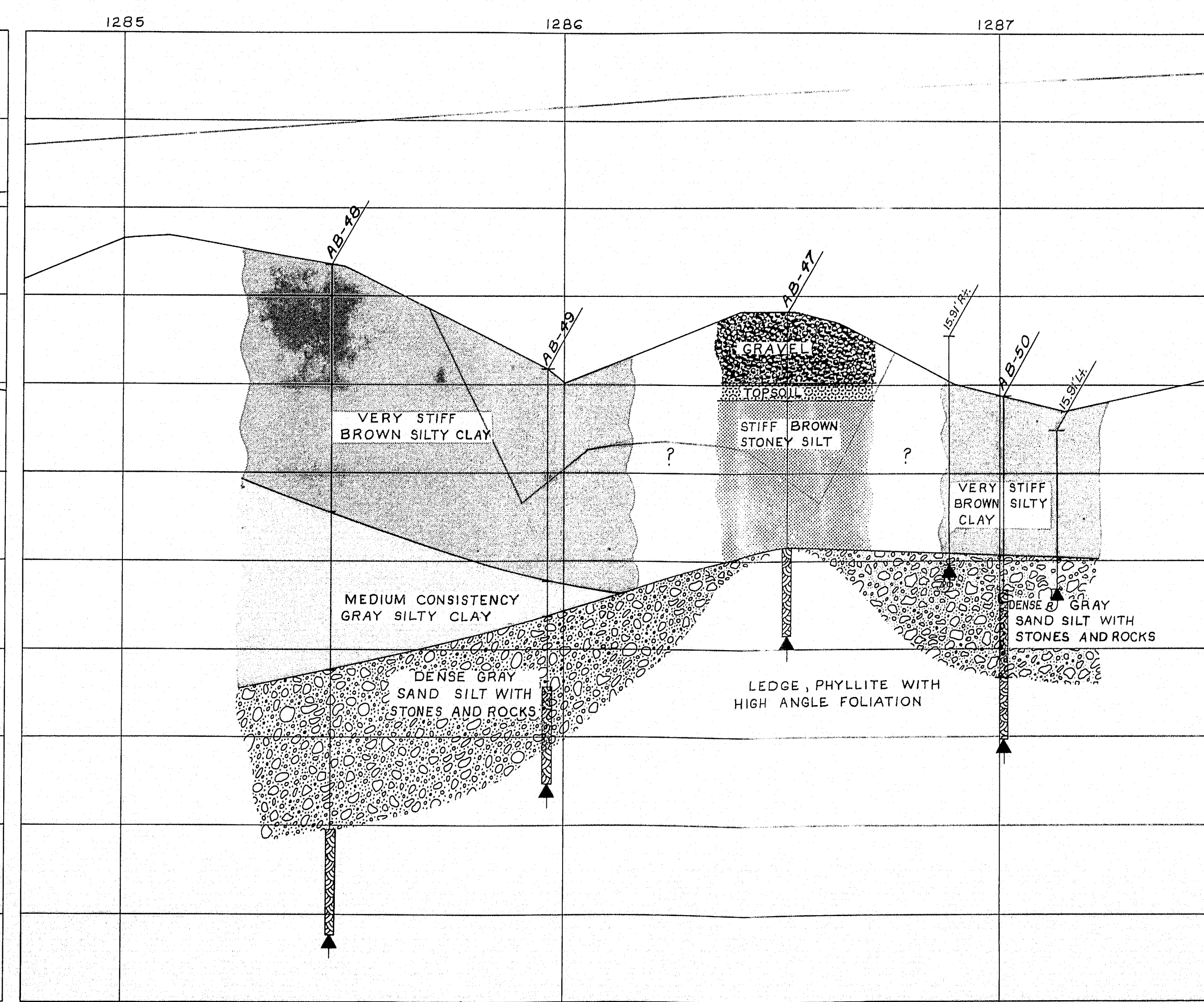
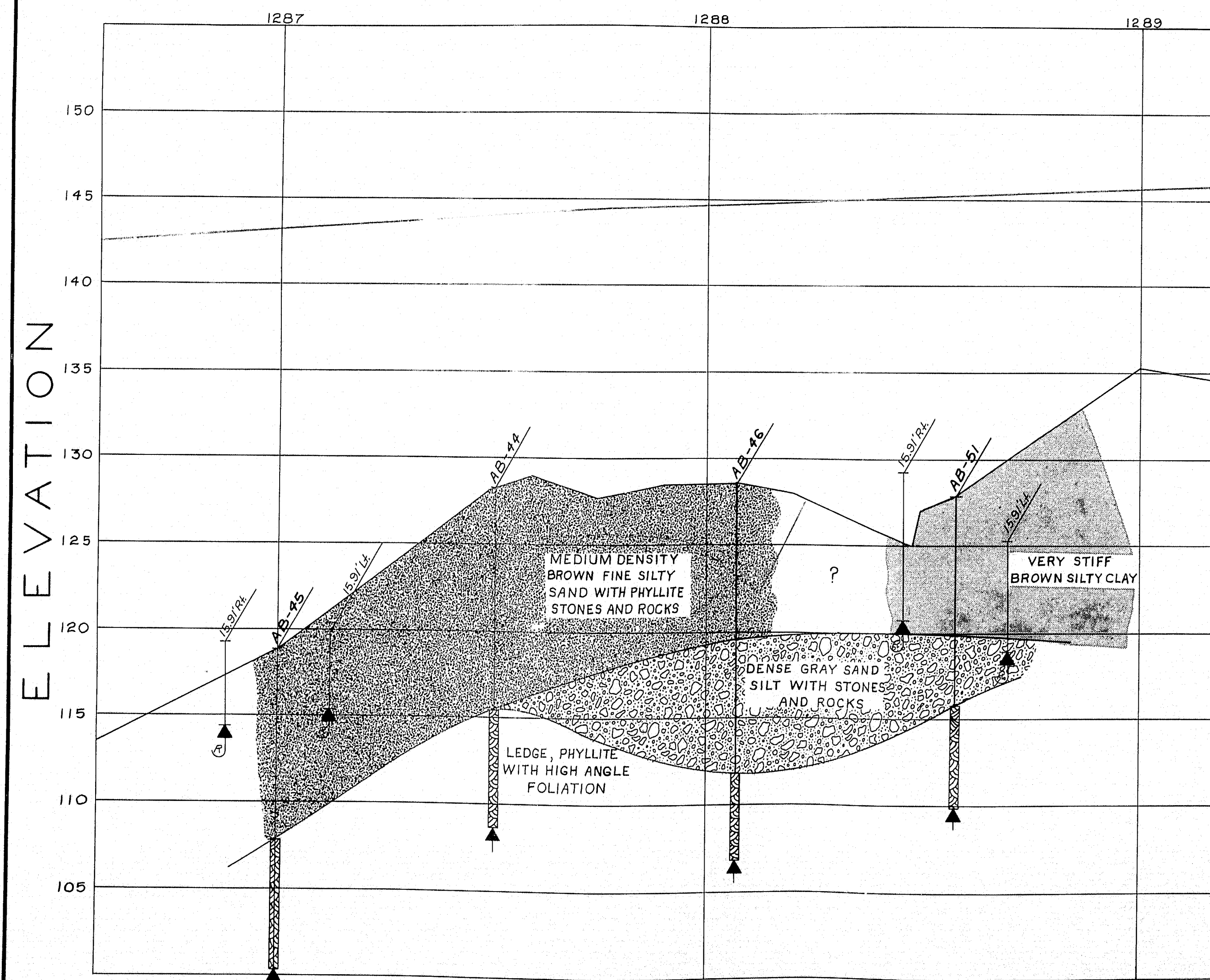
D. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	I-95-7(37)	2	29



SOUTHBOUND LANE



NORTHBOUND LANE



DESIGN—
TRACE—
CHECK—

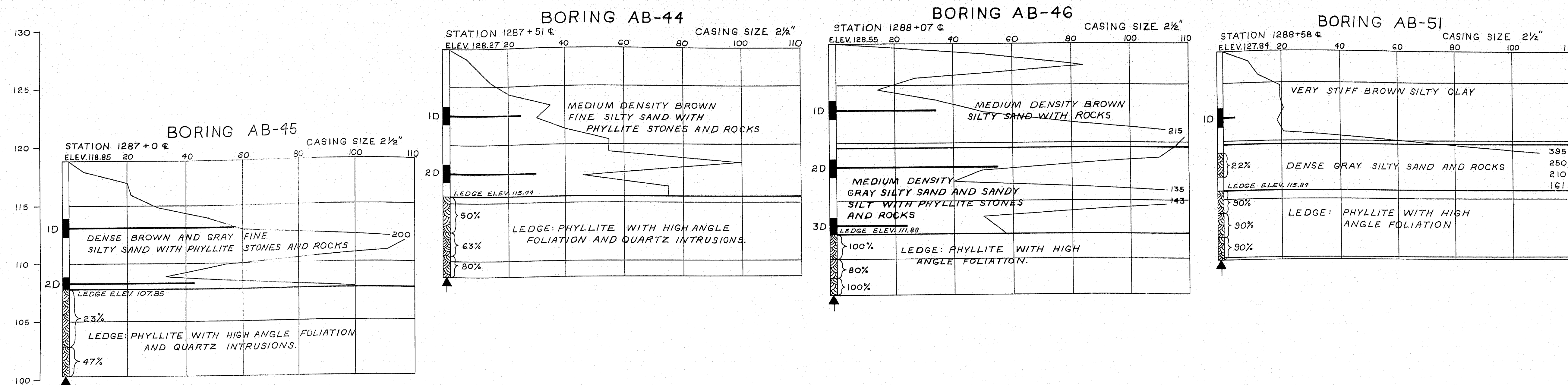
BRIDGE NO.
SURVEY—
PLOT—

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

INTERSTATE 95
OVER
RIVER ROAD
IN THE TOWN OF
BENTON
KENNEBEC COUNTY
FOUNDATION SURVEY

SHEET 2 OF 29 AUGUSTA, MAINE JUNE 1963

SOUTHBOUND LANE



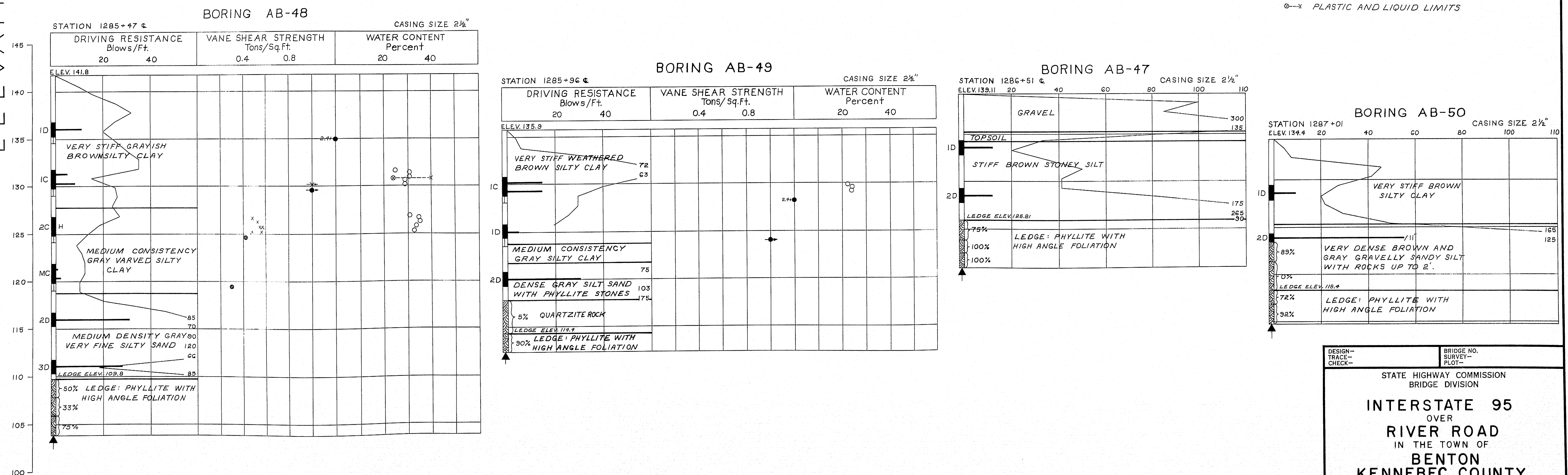
BORING NOTES
ALL SAMPLES AND VANES ARE MADE AHEAD OF CASING.
NUMBER OF BLOWS REQUIRED TO DRIVE EXTRA HEAVY CASING ONE FOOT WITH 400 FT. LBS. OF ENERGY PER BLOW.
LOCATION OF SAMPLE OR SAMPLE ATTEMPT. NUMBER AND TYPE OF DRY SAMPLE.
S&H SAMPLER #1290'S
2" O.D. 16 GA. SEAMLESS TUBING
UNSUCCESSFUL SAMPLE ATTEMPT AND TYPE OF SAMPLER.
NUMBER OF BLOWS REQUIRED TO DRIVE SPOON OR TUBING ONE FOOT WITH 350 FT. LBS. OF ENERGY PER BLOW.
SAMPLING SPOON OR SEAMLESS TUBING DRIVEN BY STATIC WEIGHT OF DRILL RODS AND HAMMER.
FIELD VANE TEST
BOTTOM OF BORING (MAY NOT BE BOTTOM OF SOIL STRATA).
LOCATIONS CORED BY DIAMOND BIT AND PER CENT RECOVERY OF ROCK.

SHEAR NOTES
• FIELD VANE SHEAR STRENGTHS
X LABORATORY VANE SHEAR STRENGTHS
→ SHEAR STRENGTHS IN EXCESS OF CAPACITY OF EQUIPMENT
○ ONE HALF UNCONFINED COMPRESSIVE STRENGTHS.

WATER CONTENT NOTES
○ NATURAL WATER CONTENTS, GIVEN AS PER CENT OF DRY WEIGHT.
○-X PLASTIC AND LIQUID LIMITS

NORTHBOUND LANE

ELEVATION



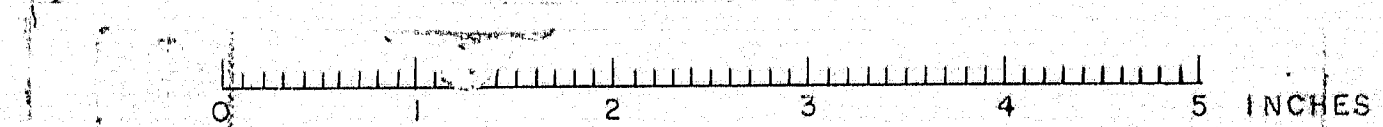
DESIGN-
TRACE-
CHECK-

BRIDGE NO.
SURVEY-
PLOT-

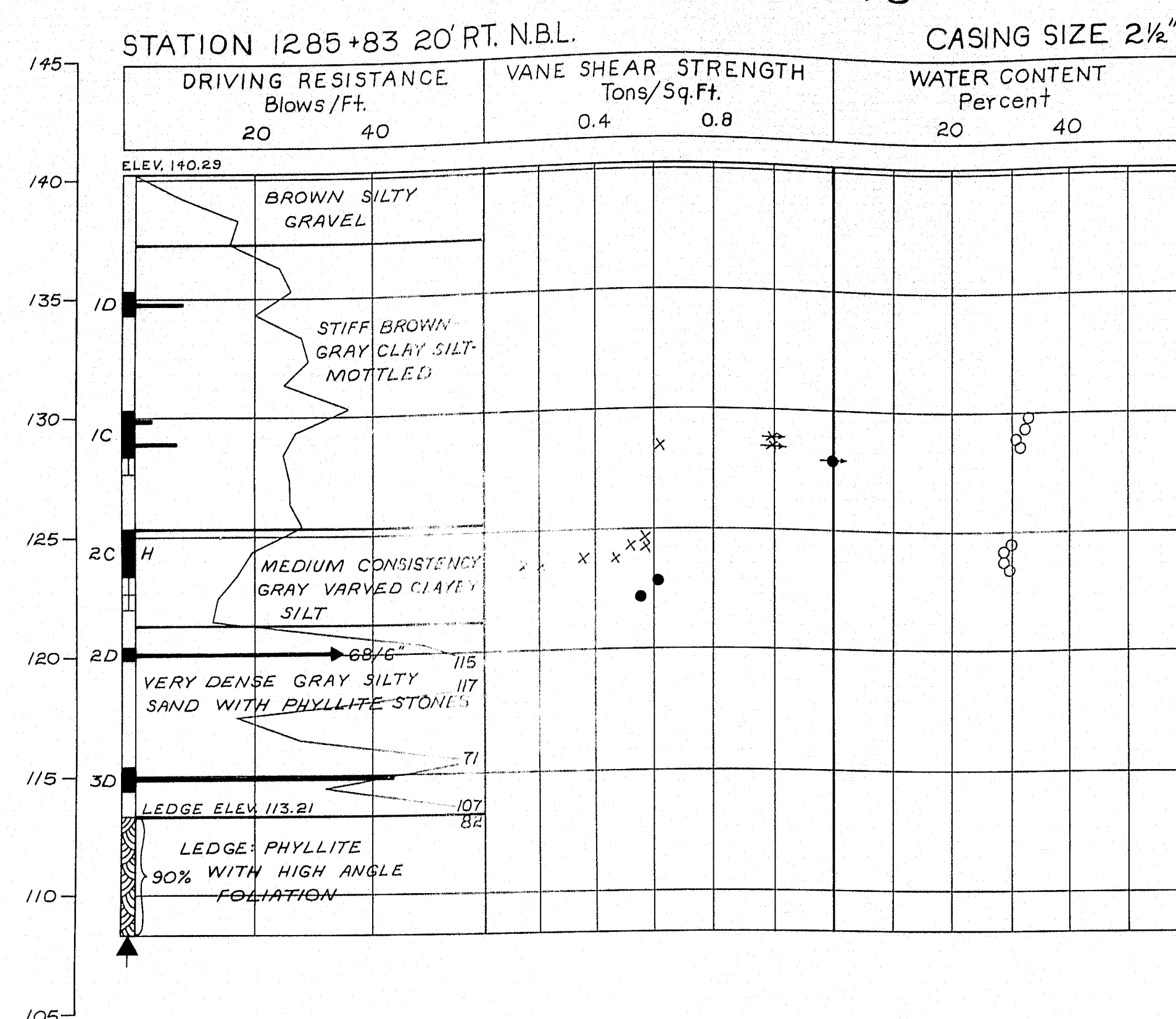
STATE HIGHWAY COMMISSION
BRIDGE DIVISION

INTERSTATE 95
OVER
RIVER ROAD
IN THE TOWN OF
BENTON
KENNEBEC COUNTY
BORING DETAILS

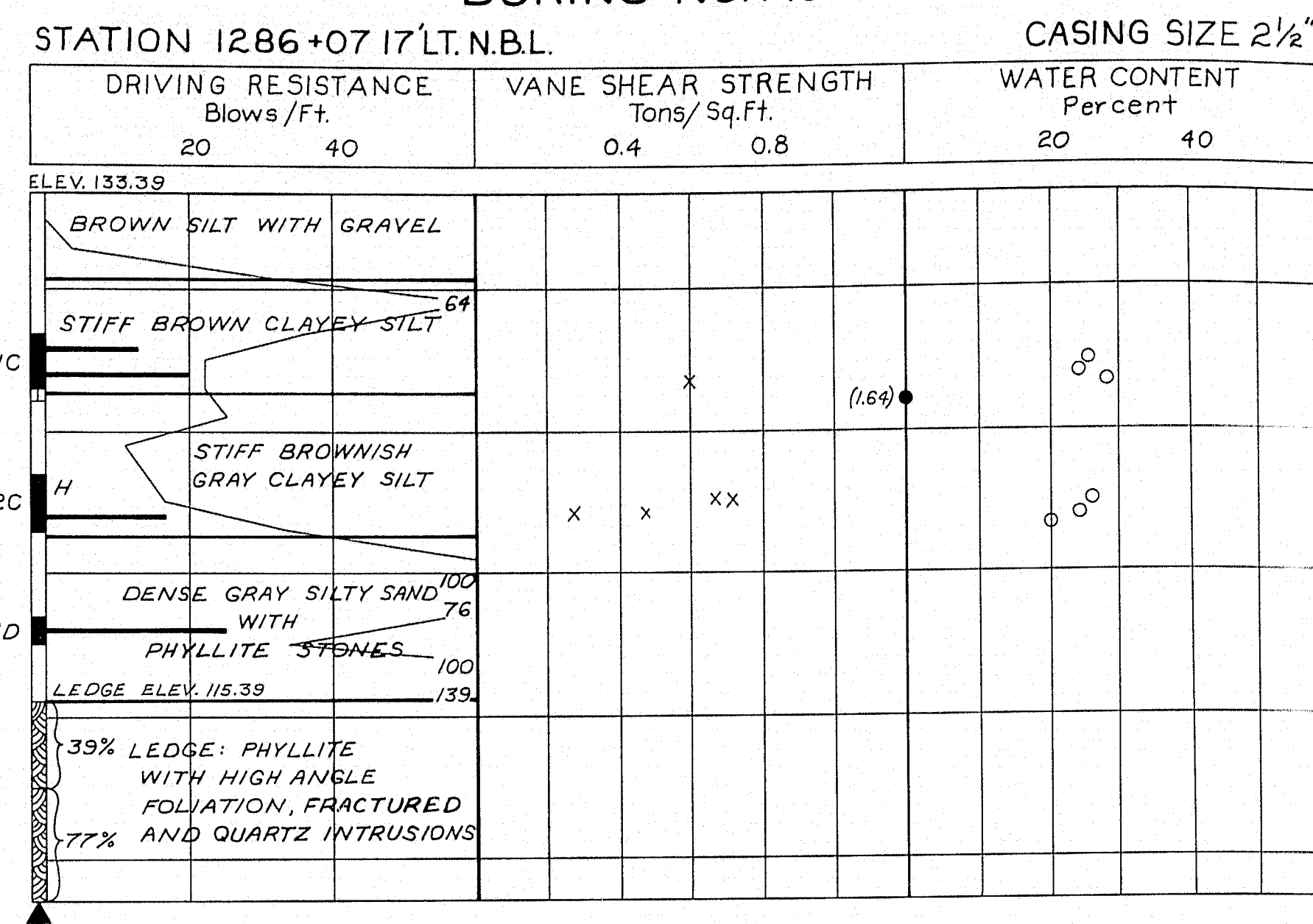
SHEET 3 OF 29 AUGUSTA, MAINE JUNE 1963



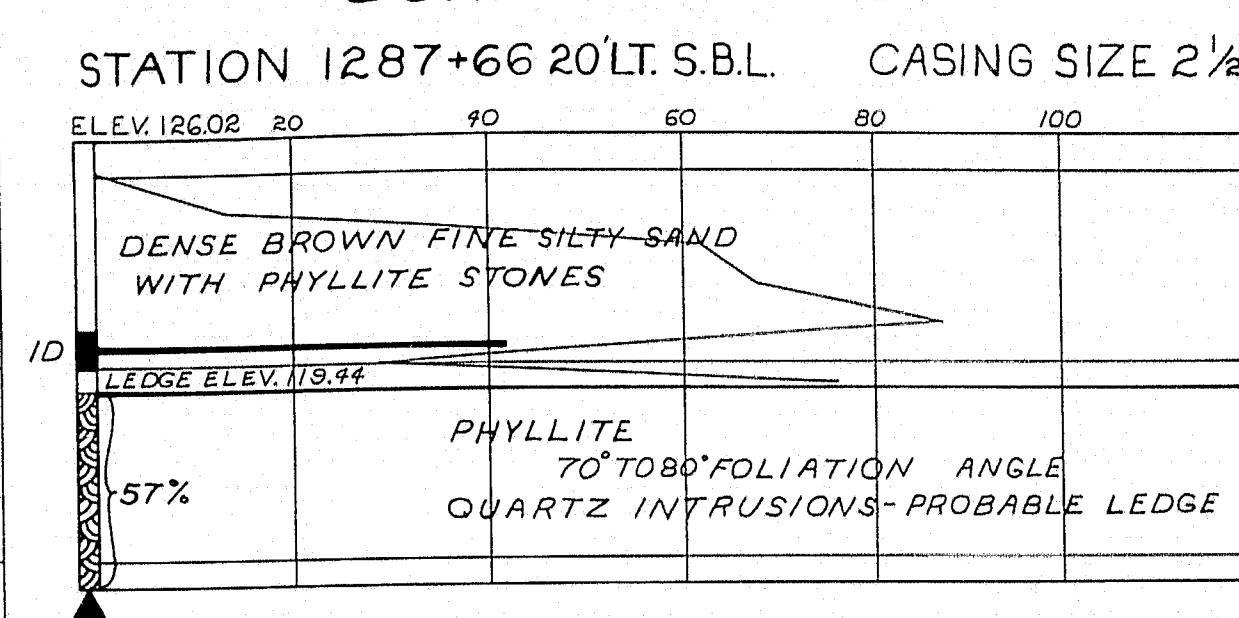
BORING NO. AC-78



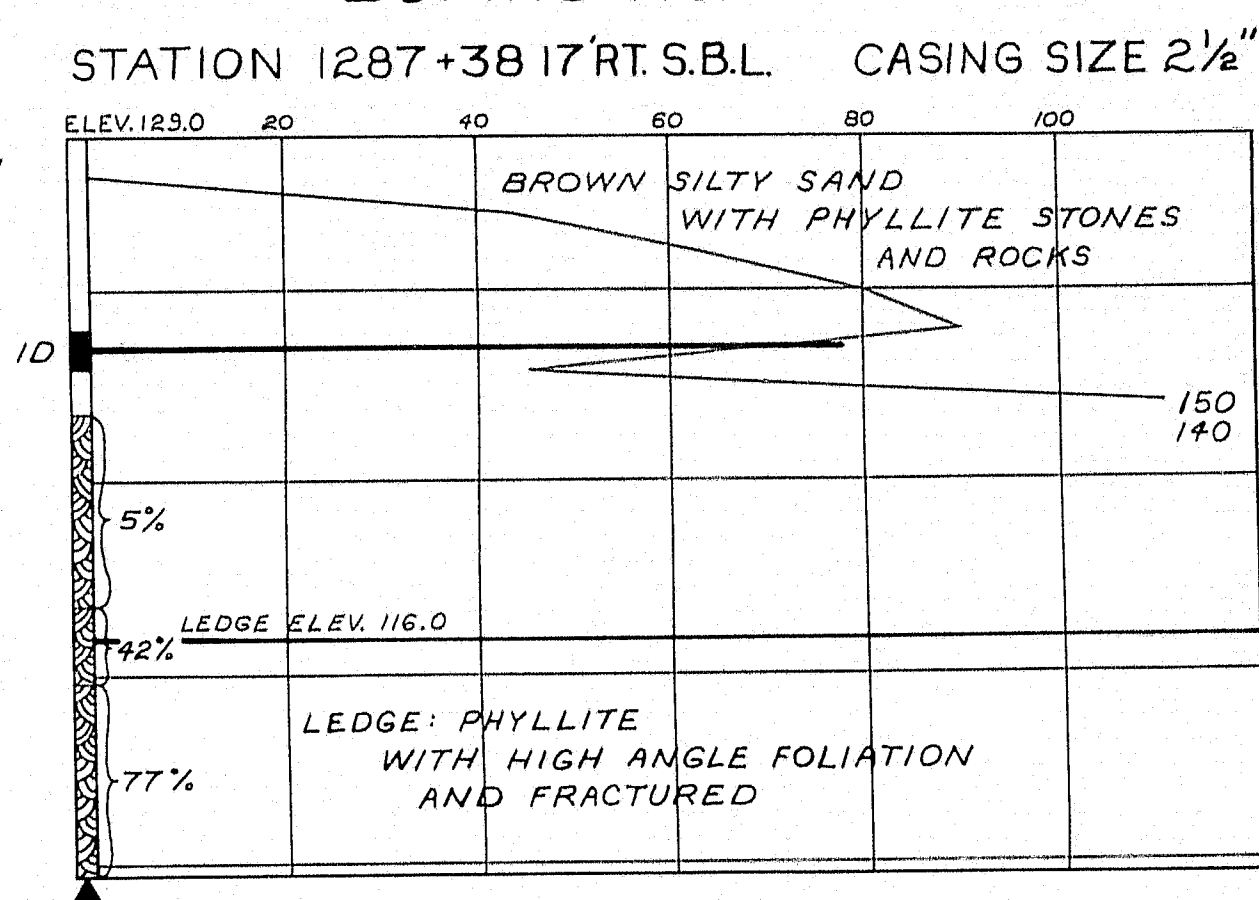
BORING NO. AC-79



BORING NO. AC-80

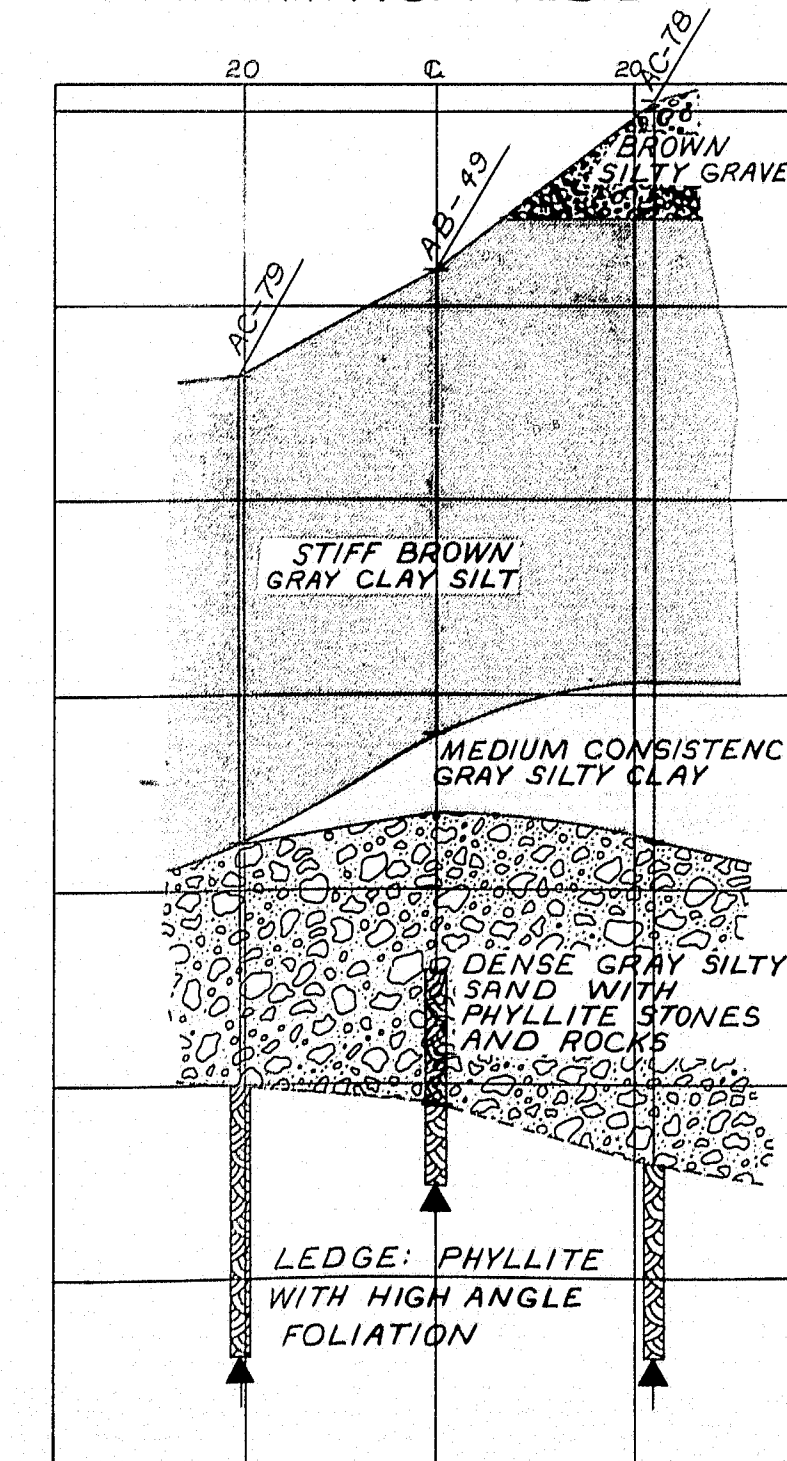


BORING NO. AC-82

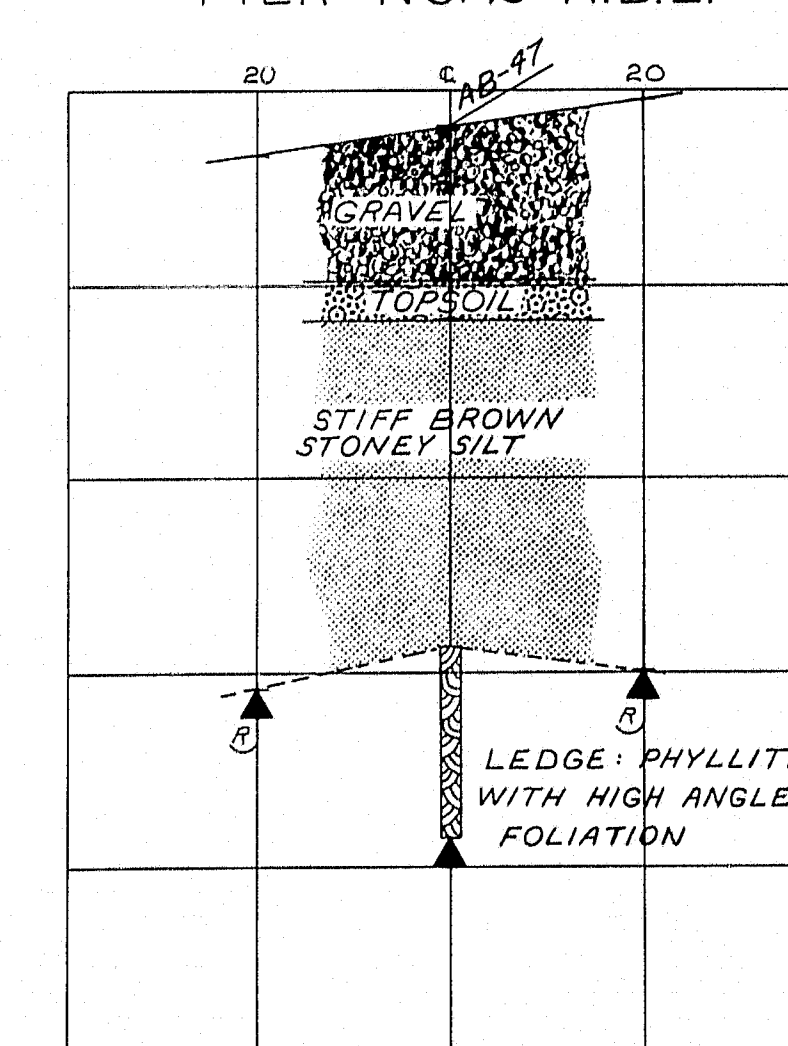


TRANSVERSE SECTIONS

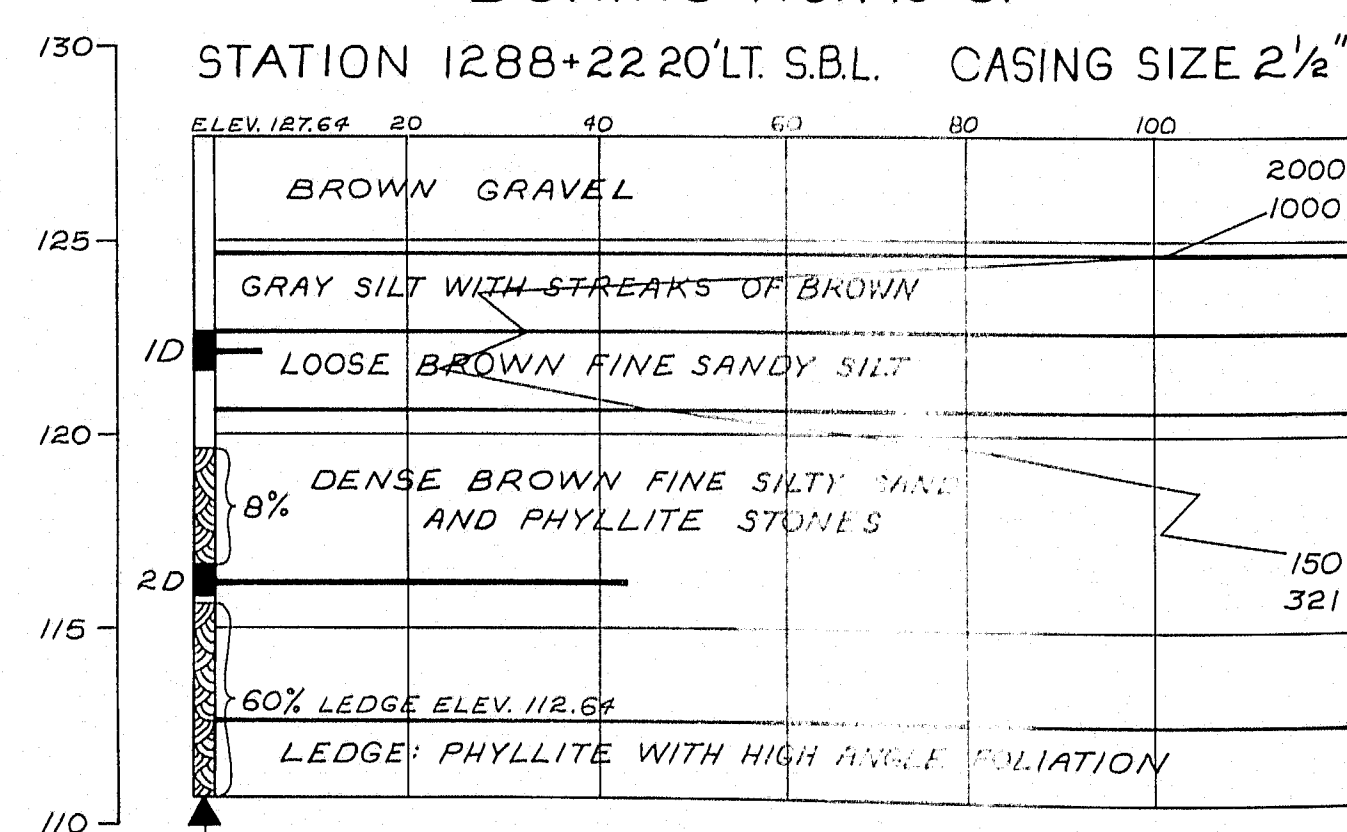
PIER NO. 1 N.B.L.



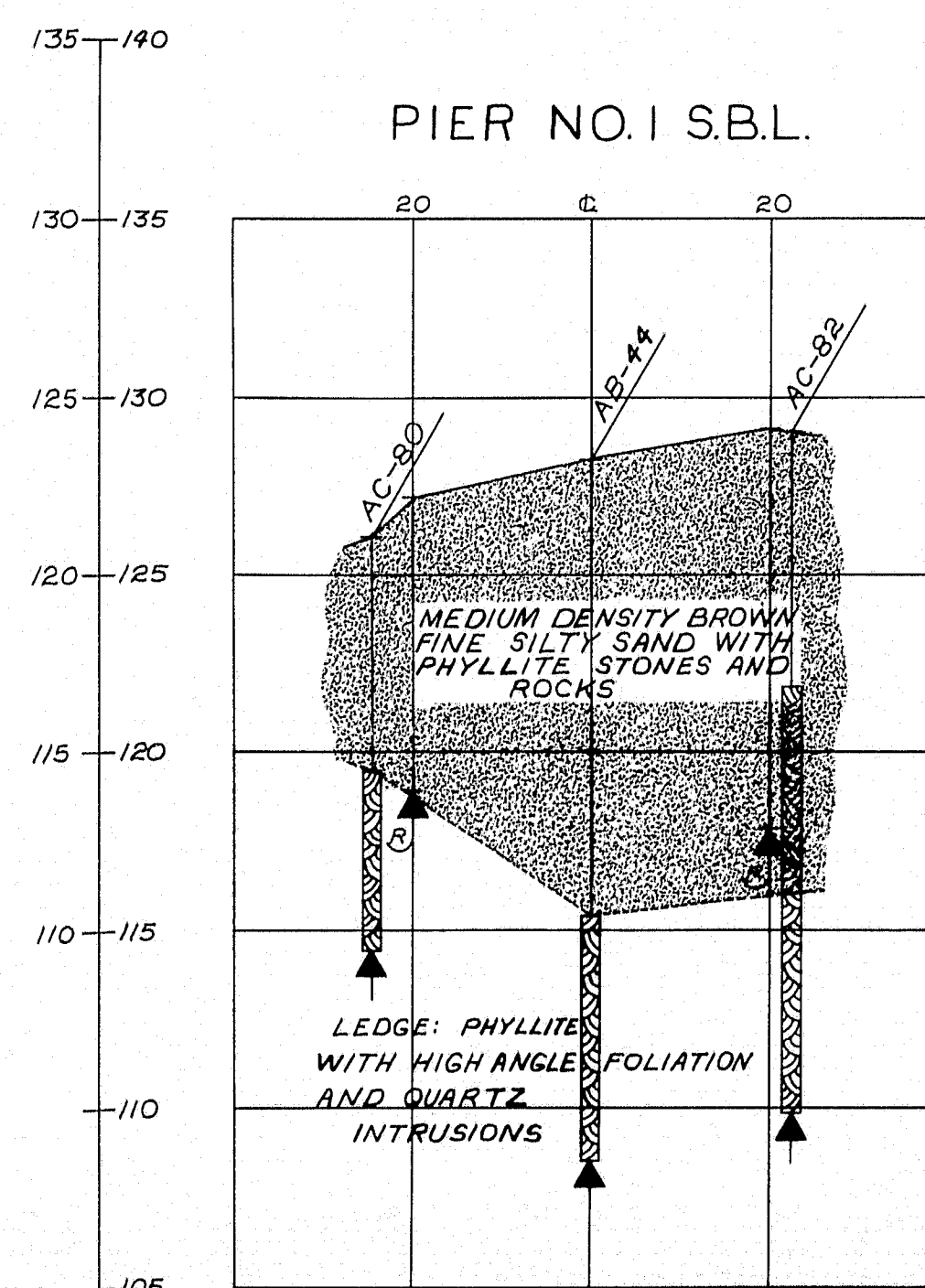
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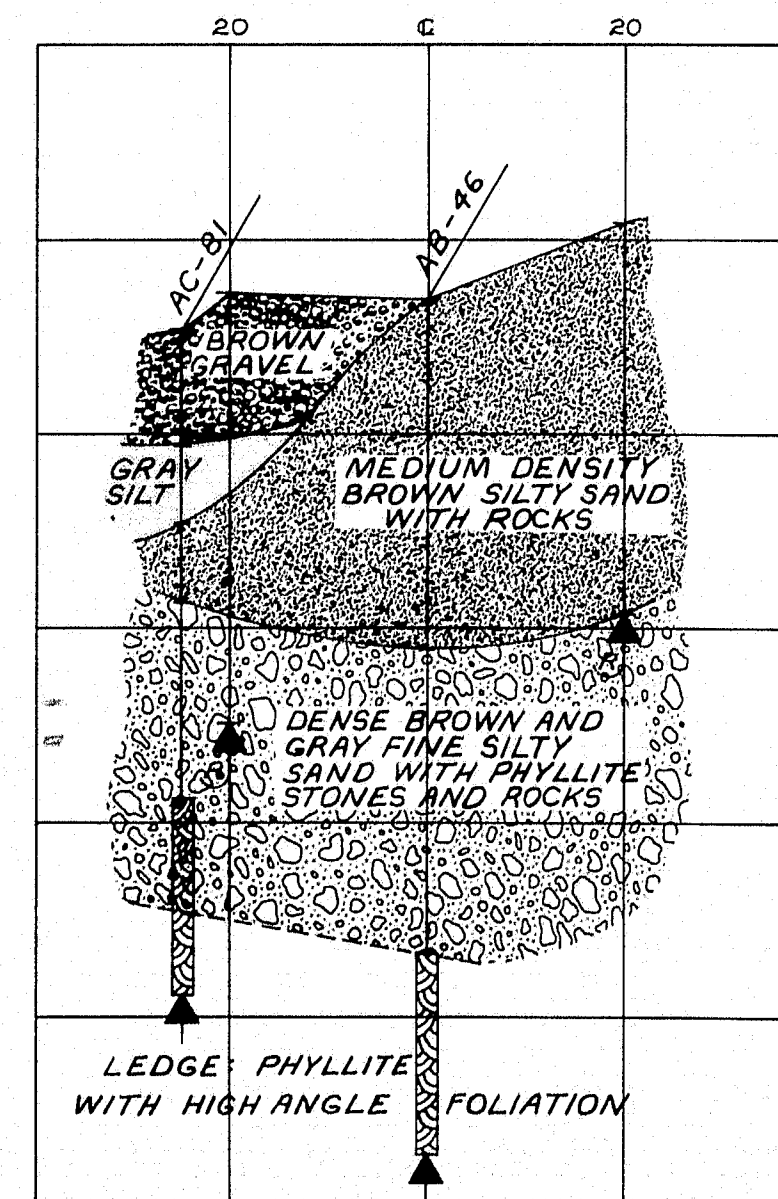
BORING NO. AC-81



PIER NO. 1 S.B.L.



PIER NO. 2 S.B.L.



DESIGN -
TRACE -
CHECK -

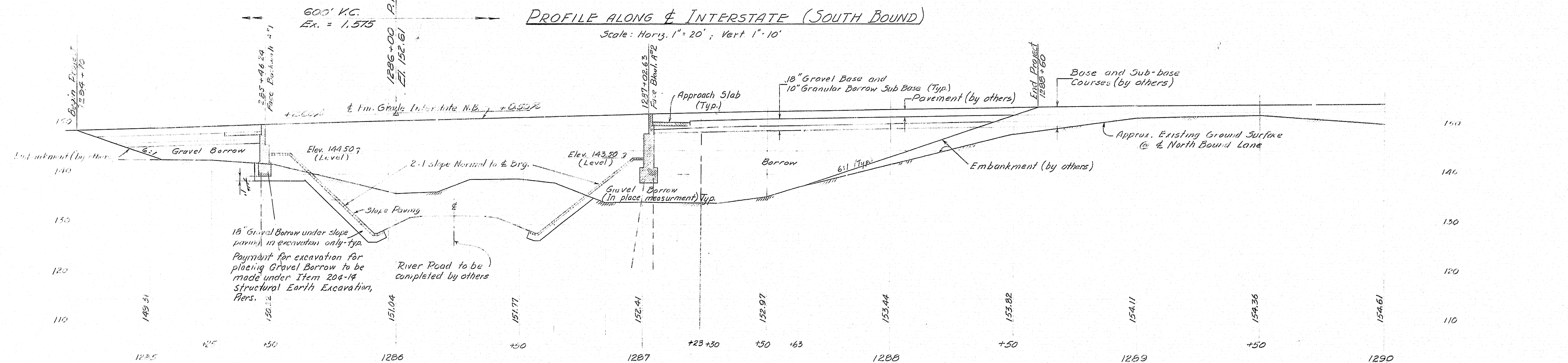
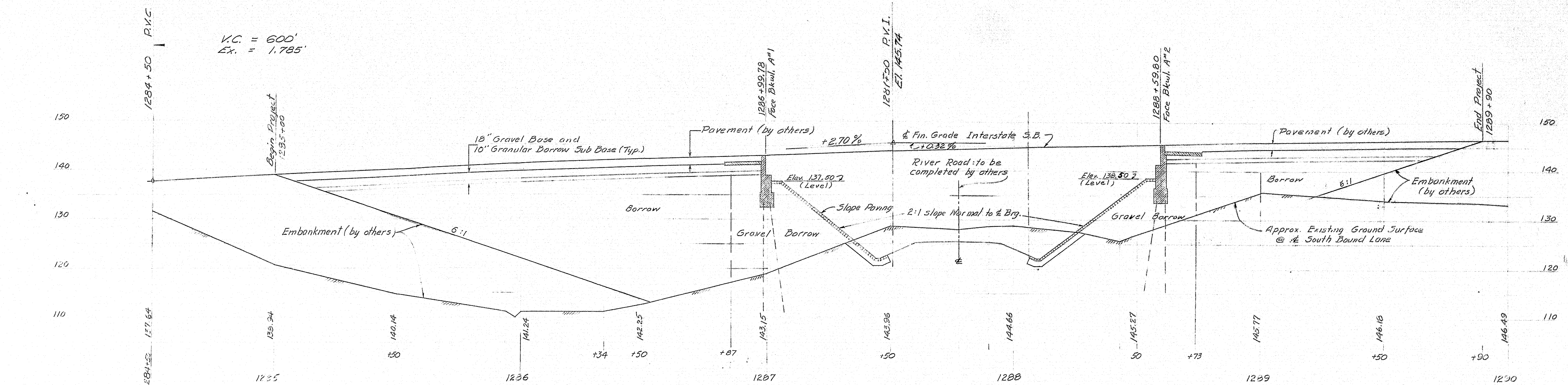
BRIDGE NO.
SURVEY -
PLOT -

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

INTERSTATE 95
OVER
RIVER ROAD
IN THE TOWN OF
BENTON
KENNEBEC COUNTY
BORING DETAILS

SHEET 4 OF 29 AUGUSTA, MAINE JUNE 1963

S.R. & S.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-7(17)	6	28

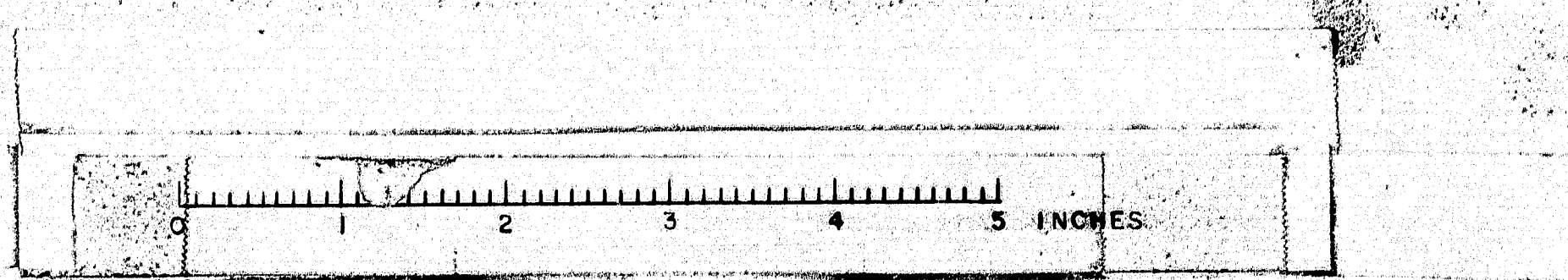


Note: As built revisions shown in Black Ink. 9-24-64 E. Barnard

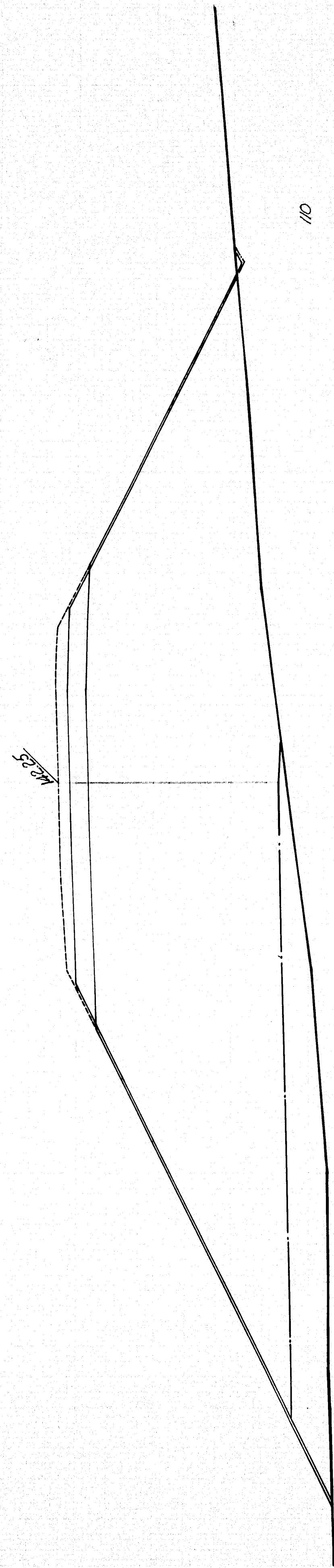
NOTES:

1. Place gravel borrow to bottom of abutment footings before driving piles.
2. Loom, Seeding Method No. 2, Hay Mulch & Asphalt Mulch Binder up to bottom of Granular Borrow South Bound Sta. 1285+00 to 1289+90, North Bound Sta. 1284+50 to 1288+60 to be done under Bridge Contract; Elsewhere by others.
3. The vertical planes at the intersection of borrow and Gravel Borrow are shown parallel to & bearings of adjacent abutments.

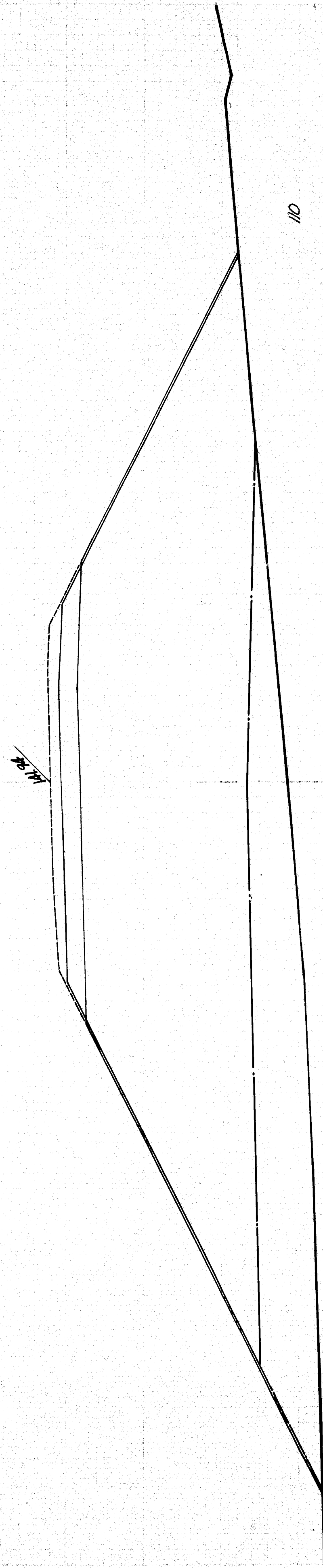
DESIGN - AMR HRD	DET - FBF	BRIDGE NO.
TRACE - FBF	CHECK - D. Barnard	DATE -
STATE HIGHWAY COMMISSION		
BRIDGE DIVISION		
INTERSTATE 95 OVER RIVER ROAD		
IN THE TOWN OF		
BENTON		
KENNEBEC COUNTY		
ROADWAY WORK, PROFILE		
SHEET 6 OF 29 AUGUSTA, MAINE JUNE 1965		



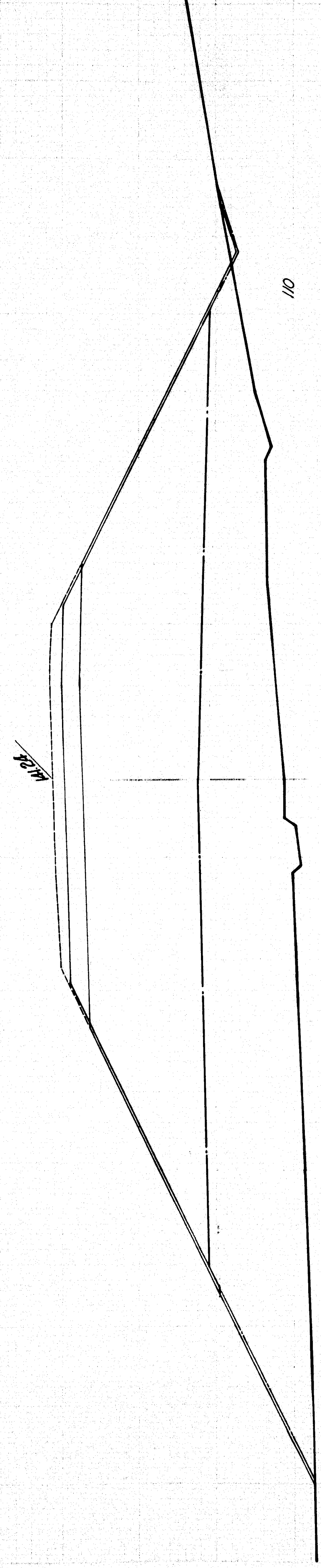
D. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-7(37)	6	29



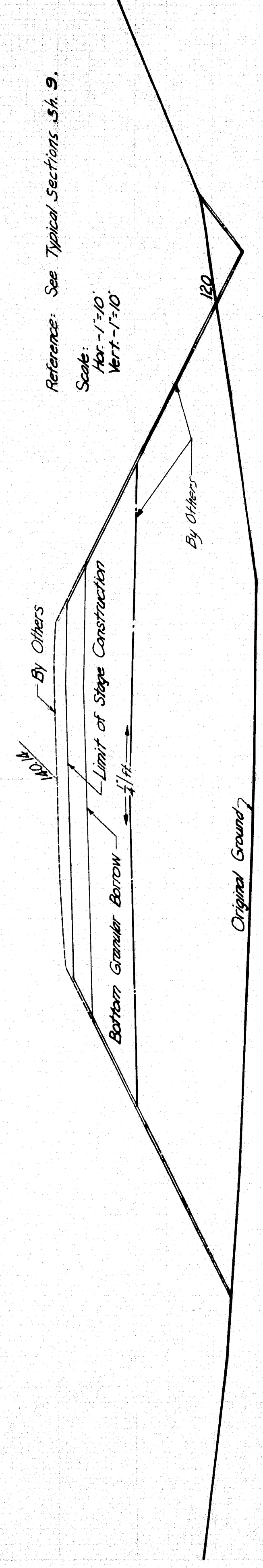
1286+50



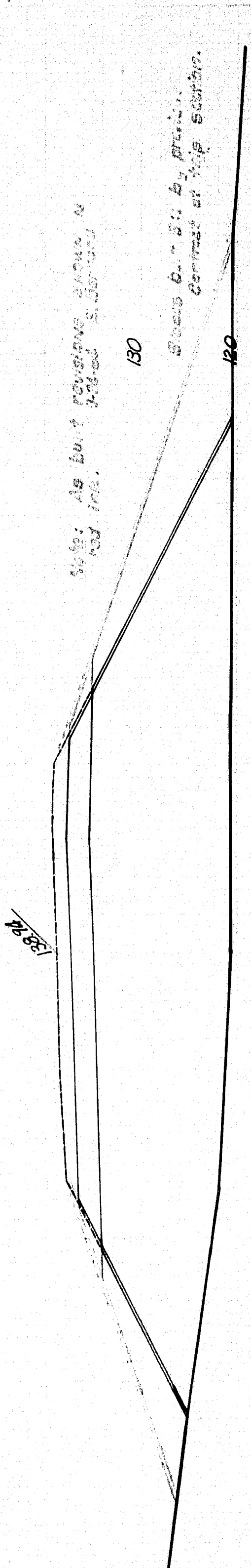
1286+34



1286+0



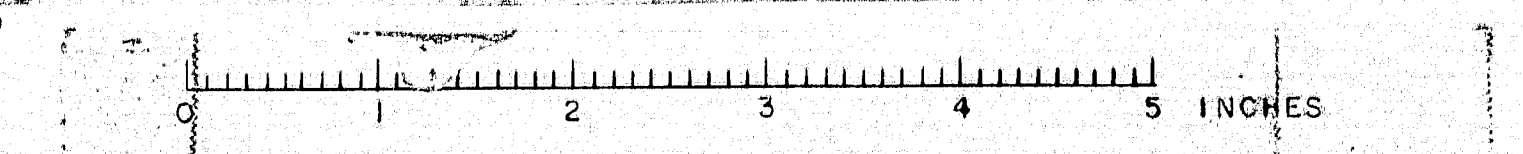
1286+50



1286+0

Reference: See Typical Sections Sh. 9.
Scale:
Hor. 1"=10'
Vert. 1"=10'

DETAIL - RGS TRACE - RGS CHECK - <i>Dressley</i>	STATE HIGHWAY COMMISSION BRIDGE DIVISION INTERSTATE 95 OVER RIVER ROAD IN THE TOWN OF BENTON KENNEBEC COUNTY CROSSECTIONS 88 SHEET 6 OF 29 AUGUSTA MAINE JUNE 1963
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D. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-7 (37)	7	29

1289+20
End Project

1289+30

130

Enbankment by others

1289+50

1291+0

130

1291+0

1288+57.80
Face Backwall
Abut #2

Bridge

1286+99.78
Face Backwall
Abut #1

1287+0

140

1287+0

1287+0

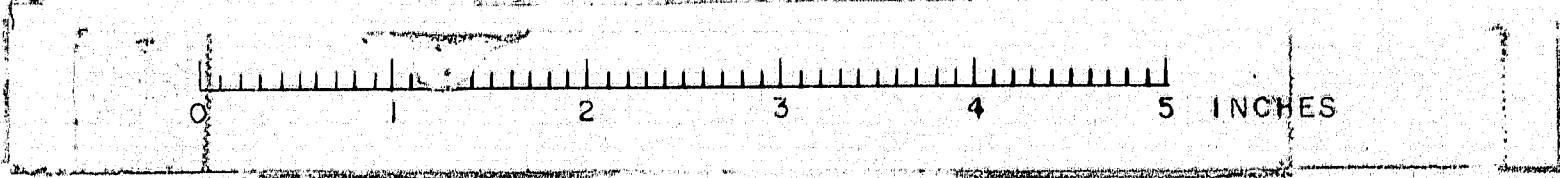
DETAIL - ROG
TRACE - ROG
CHECK - *Dressly*

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

INTERSTATE 95 OVER RIVER ROAD
IN THE TOWN OF
BENTON
KENNEBEC COUNTY

CROSSSECTIONS SB

SHEET 7 OF 29 AUGUSTA MAINE JUNE 1963



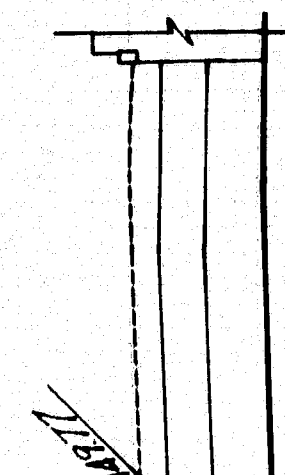
95/479

- BRIDGE -

1285+46.24 (N.B.)
Face of Backwall
About No. 1

1285+25

130



DETAIL - ROG	
TRACE - ROG	
CHECK - <i>Donnelly</i>	
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95 OVER RIVER ROAD	
IN THE TOWN OF BENTON KENNEBEC COUNTY	
CROSSECTIONS NB	
SHEET 8 OF 29 AUGUSTA MAINE 1963	

D. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-7 (37)	8	28

Original Ground 7

Arch 140
by others
(typical)

Limit of Stage Construction

1285+00

By Others

Gravel Barrow

Bottom Gravel Barrow

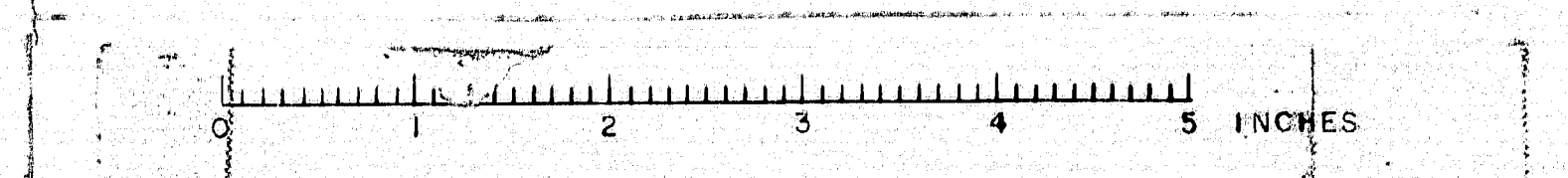
End of Project

130 Reference: See Typical Sections Sh. 9.

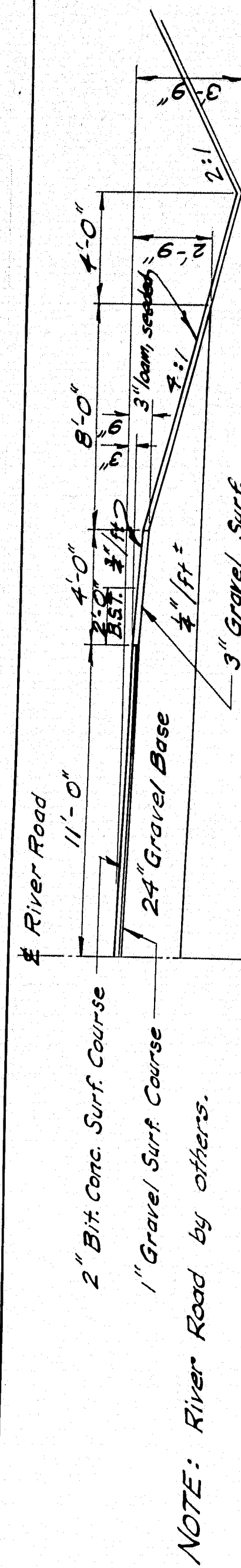
Scale:
Hor. 1"=10'
Vert. 1"=10'

1284+70
Begin Project

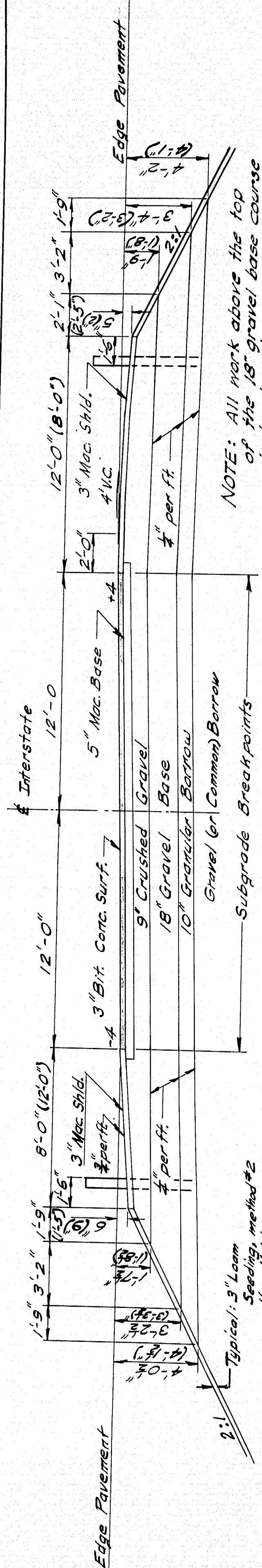
4 (N.B.)



95/479

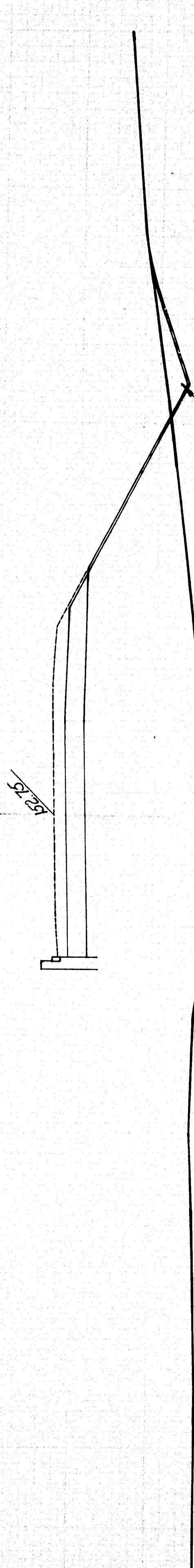
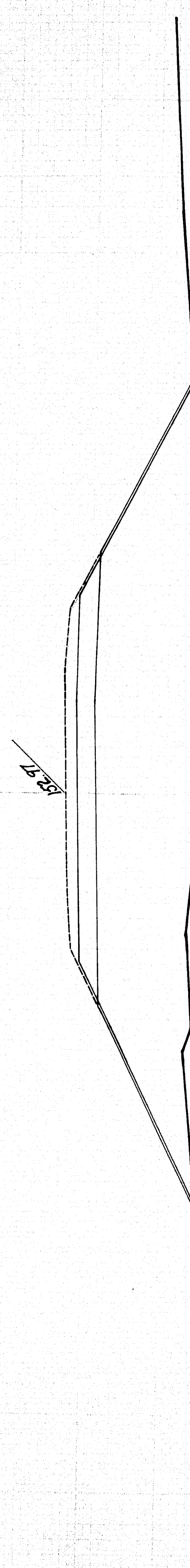
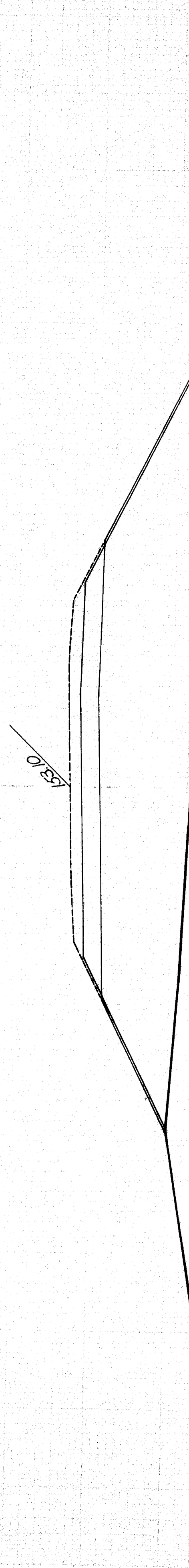
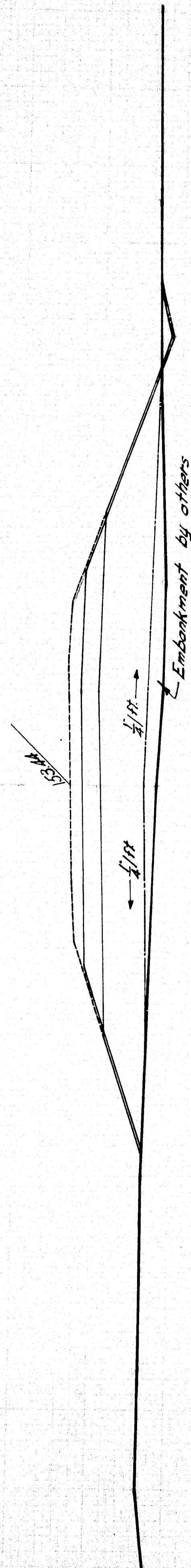
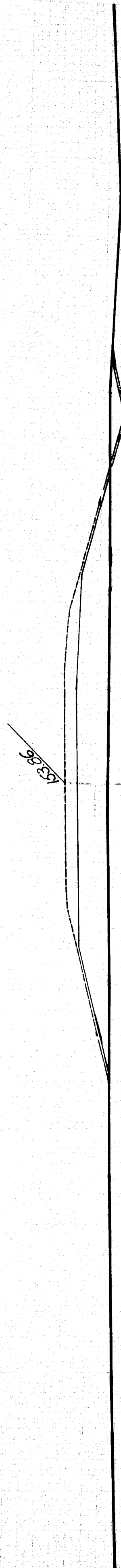


~ TYPICAL RIVER ROAD SECTION ~



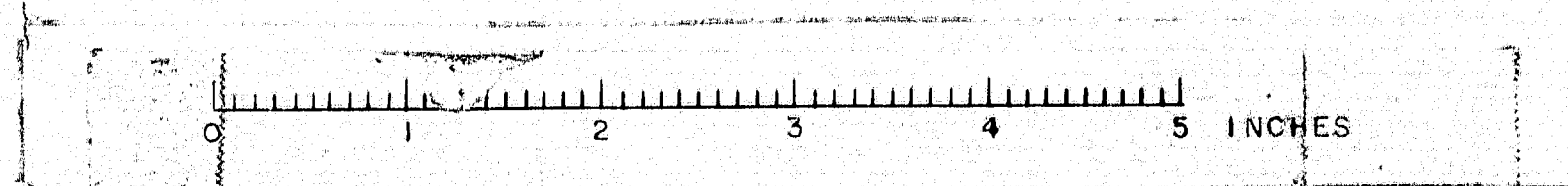
~ TYPICAL INTERSTATE SECTION-NB ~

SB, same except where separate dimensions in parentheses are shown. See Sheet 5 for limits of work done under this contract and material limits.



DETAIL - ROG	STATE HIGHWAY COMMISSION
TRACE - ROG	BRIDGE DIVISION
CHECK - D. Seelye	
INTERSTATE 95 OVER RIVER ROAD	
IN THE TOWN OF	
BENTON	
KENNEBEC COUNTY	
CROSSSECTIONS NB	
TYPICAL SECTIONS	
SHEET 9 OF 29 AUGUSTA MAINE JUNE 1963	

1287+02.63 (NB)
Face of Backwall
Abut No. 2
(NB)



CURVE DATA

Horizontal	Vertical
Southbound PVI Sta 1287+36.85 L = 40'-23'-26" LT D = 1°-18' T = 1621.18' L = 3106.97' R = 4407.37' E = 288.71'	PVI Sta 1287+50, El. 145.74 L = 600' E = 178.50' g ₁ = +2.70% g ₂ = +0.32% P.V.C. Sta 1284+50, El. 137.64

Horizontal	Vertical
Northbound PVI Sta 1283+49.84 L = 43'-07'-43" LT D = 1°-15' T = 1813.03' L = 3472.96' R = 4583.66' E = 345.54'	PVI Sta 1286+00, El. 152.61 L = 600' E = 157.50' g ₁ = +2.60% g ₂ = 0.60% P.V.C. Sta 1283+00, El. 144.81

River Road

Vertical
 PVI Sta 17+90, El. 117.48
 P.V.C. Sta 14+05, El. 133.73
 L = 650'
 E = 8.1230
 g₁ = -5.00%
 g₂ = +5.00%

UTILITIES

Central Maine Power Company
 New England Tel. & Tel. Company

ESTIMATE OF QUANTITIES

ITEM DESCRIPTION	UNIT	TOTAL QUANTITIES	BRIDGE QUANTITIES
Structural Earth Excavation-Abutments & Retaining Walls	Cu. Yds.	50	50
Structural Earth Excavation-Piers	Cu. Yds.	700	700
Borrow	Cu. Yds.	13,600	
Granular Borrow	Cu. Yds.	930	
Gravel Borrow-In Place Measurement	Cu. Yds.	5,500	
Gravel Base Course-In Place Measure	Cu. Yds.	1,570	
* Bituminous Concrete Surface Course-Type "A"	Ton	154	154
Portland Cement Concrete-Abutments & Retaining Walls	Cu. Yds.	464	469
Portland Cement Concrete-Piers	Cu. Yds.	283	283
Portland Cement Concrete-Roadway Sidewalk Slab-On Steel Bridges	Cu. Yds.	385	385
Portland Cement	Bbls.	1,700	1,700
Structural Steel-Fabricated and Delivered	L. S.	Lump Sum	Lump Sum
Structural Steel-Erection	L. S.	Lump Sum	Lump Sum
Structural Steel-Field Painting	L. S.	Lump Sum	Lump Sum
Reinforcing Steel-Delivered	Lbs.	166,400	166,400
Reinforcing Steel-Placing	Lbs.	166,400	166,400
Steel H-Beam Piles, 42 lbs./ft.	Lin. Ft.	1,735	1,735
Bridge Rail	Lin. Ft.	706	706
* Membrane Waterproofing	Sq. Yd.	1,405	1,405
Epoxy Resin Surface Sealant	Sq. Yd.	233	233
Slope Paving	Sq. Yd.	1,070	
Granite Bridge Curb	Lin. Ft.	728	728
Loam Borrow	Cu. Yds.	600	
Seeding-Method No. 2	Unit	65	
Hay Mulch	Ton	3.5	
Asphalt Mulch Binder	Gallon	1,800	

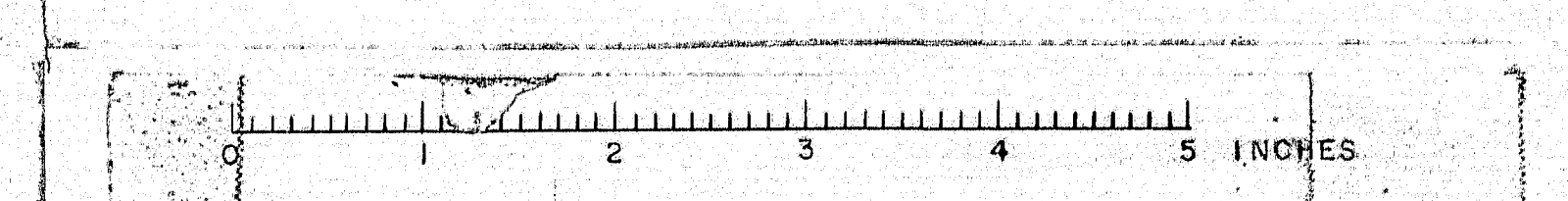
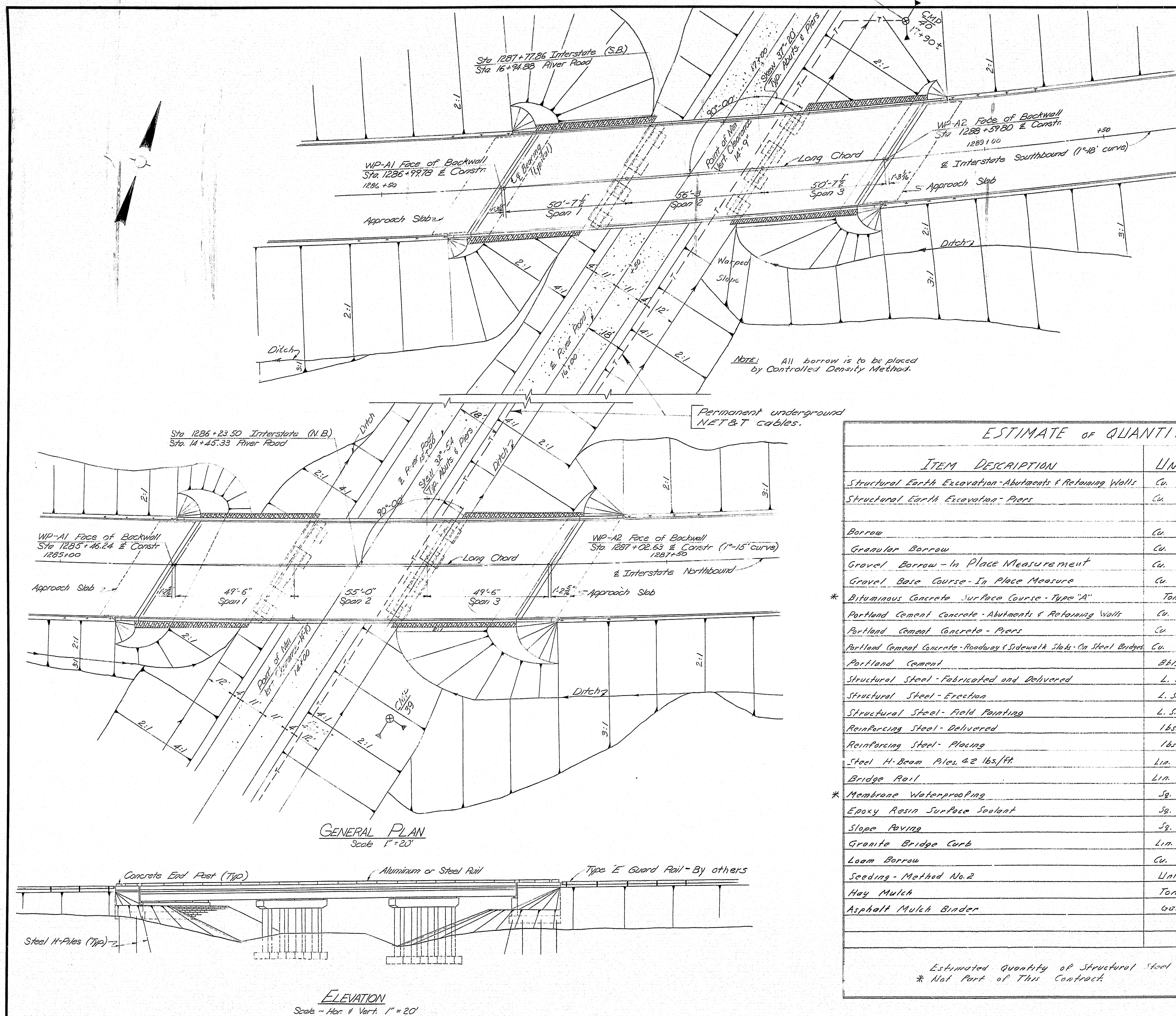
Estimated Quantity of Structural Steel = 296,600 lbs.
 * Not Part of This Contract

SPECIFICATIONS

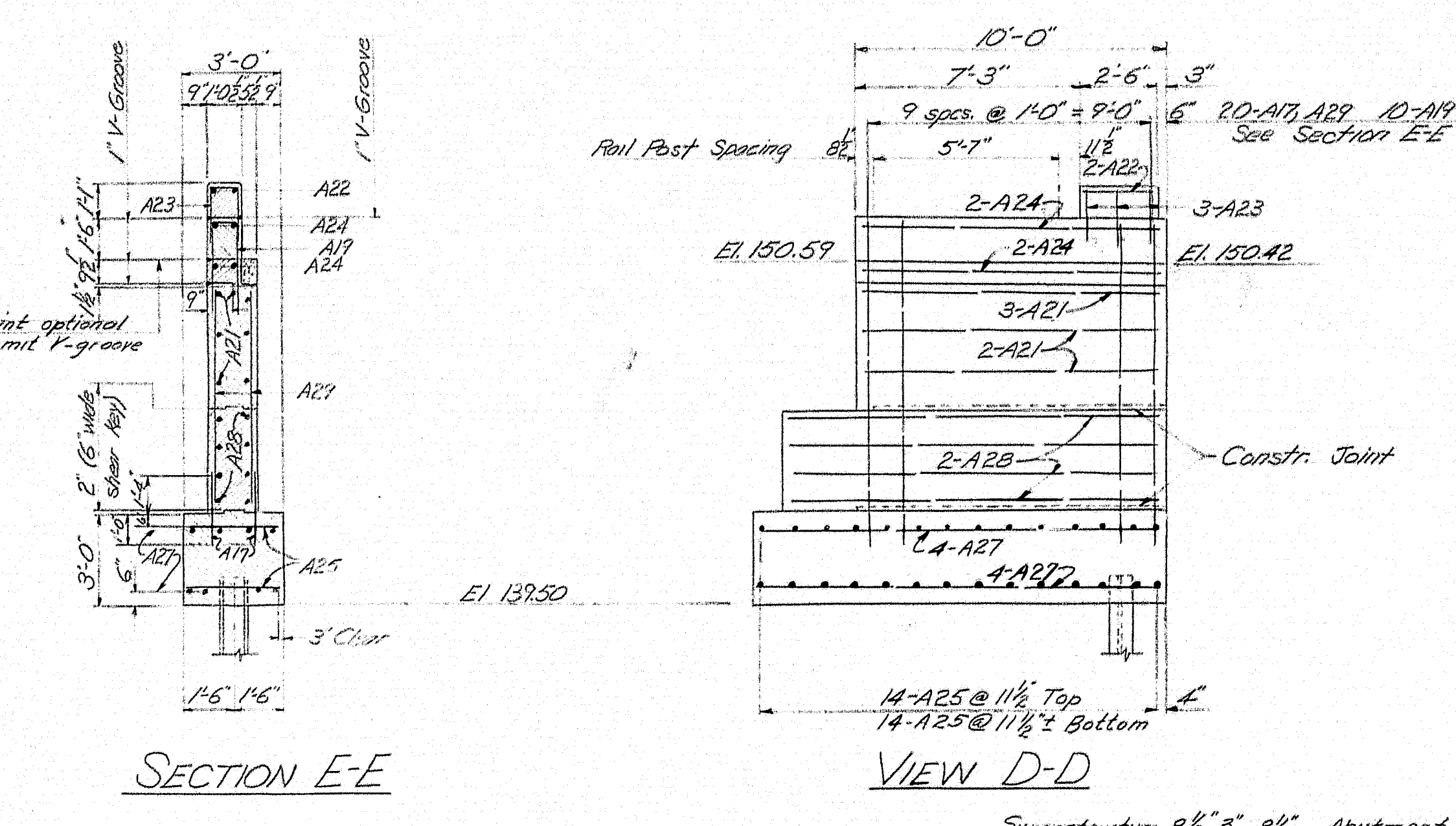
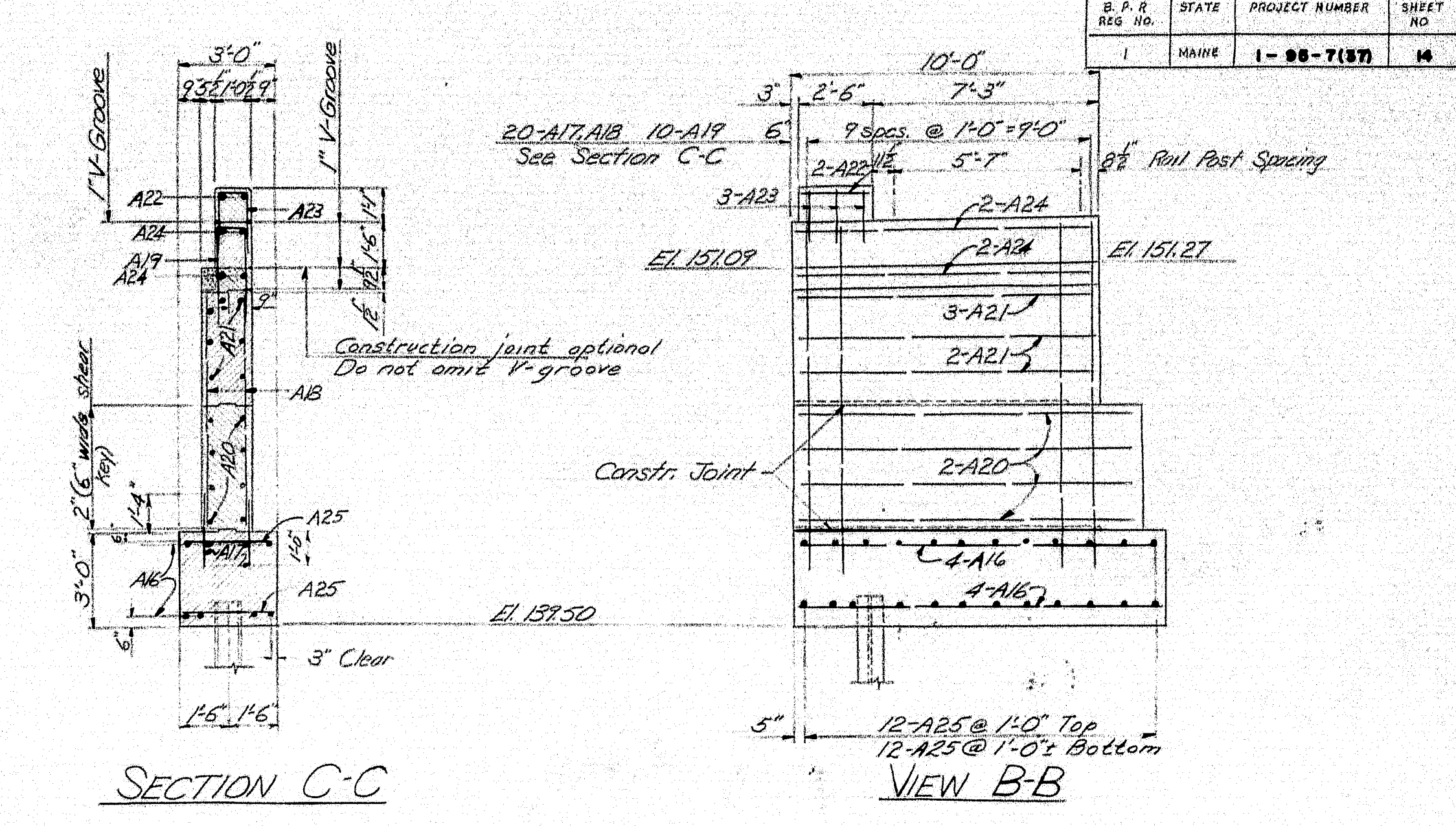
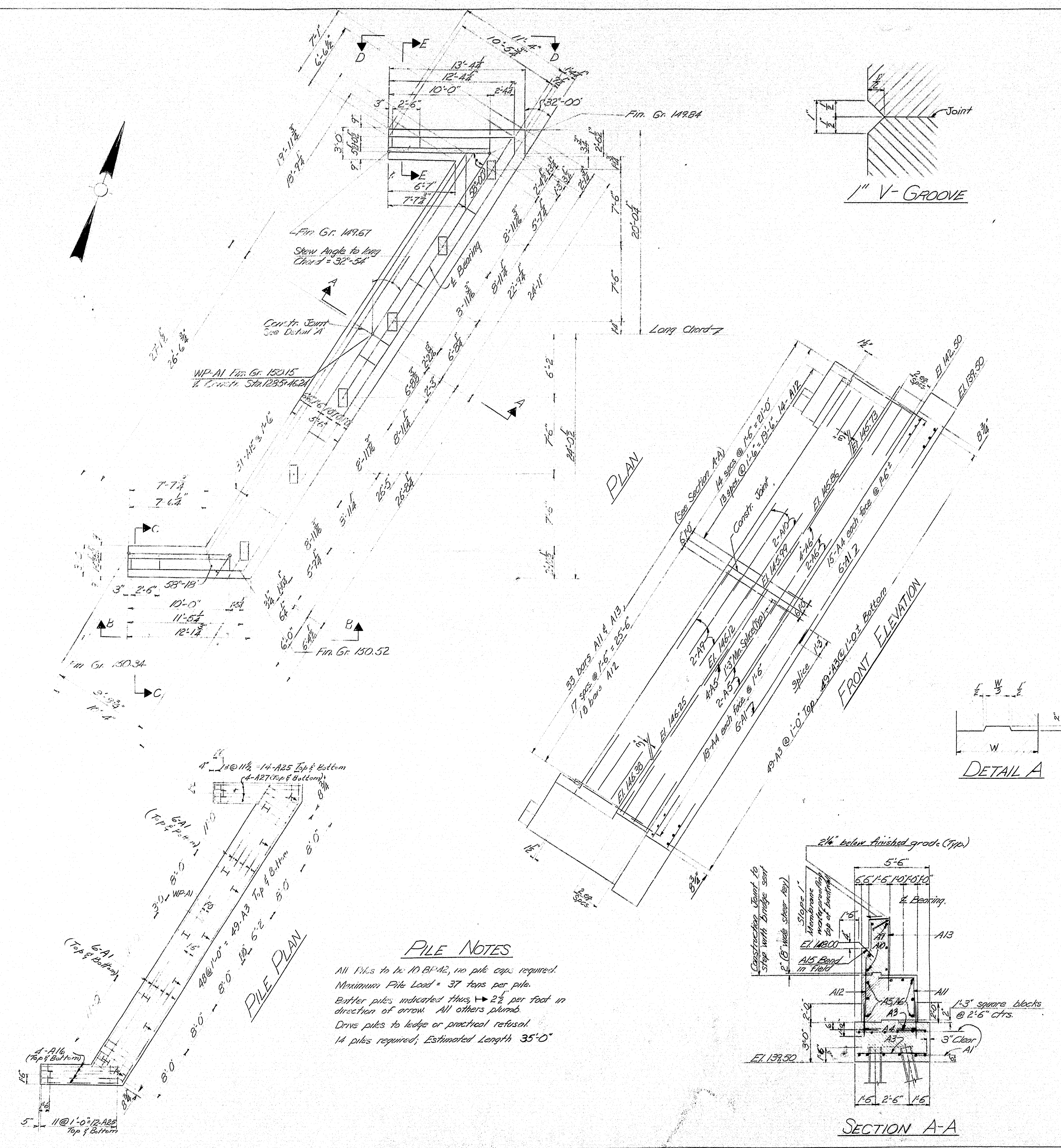
DESIGN: A.A.S.H.O. Standard Specifications for Highway Bridges 1961, with interim revisions.
 CONSTRUCTION: State of Maine, State Highway Comm. Standard Specifications for Highways and Bridges, revision of January 1955, with supplements.
 LOADINGS: H20-S16-44, modified for Interstate Structural Steel-13k16, 20,000, A7-L-18,000 Reinforcing Steel - F_y = 80,000 psi. Concrete - (n=10), f_c = 1,800 psi.

CONCRETE CLASSIFICATIONS: All concrete Class "A"

DETAIL-ROB TRACE-ROB CHECK-Drawn	BRIDGE NO. SURVEY- PLAT-
STATE HIGHWAY COMMISSION BRIDGE DIVISION INTERSTATE 95 OVER RIVER ROAD IN THE TOWN OF BENTON KENNEBEC COUNTY GENERAL PLAN QUANTITIES SHEET 10 OF 29 AUGUSTA, MAINE, JUNE 1963	



B. P. & R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-7(17)	14	28

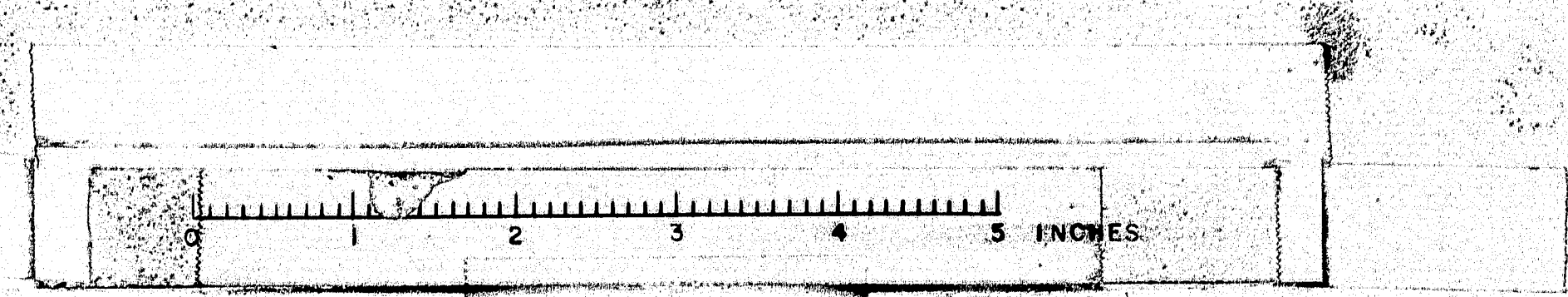


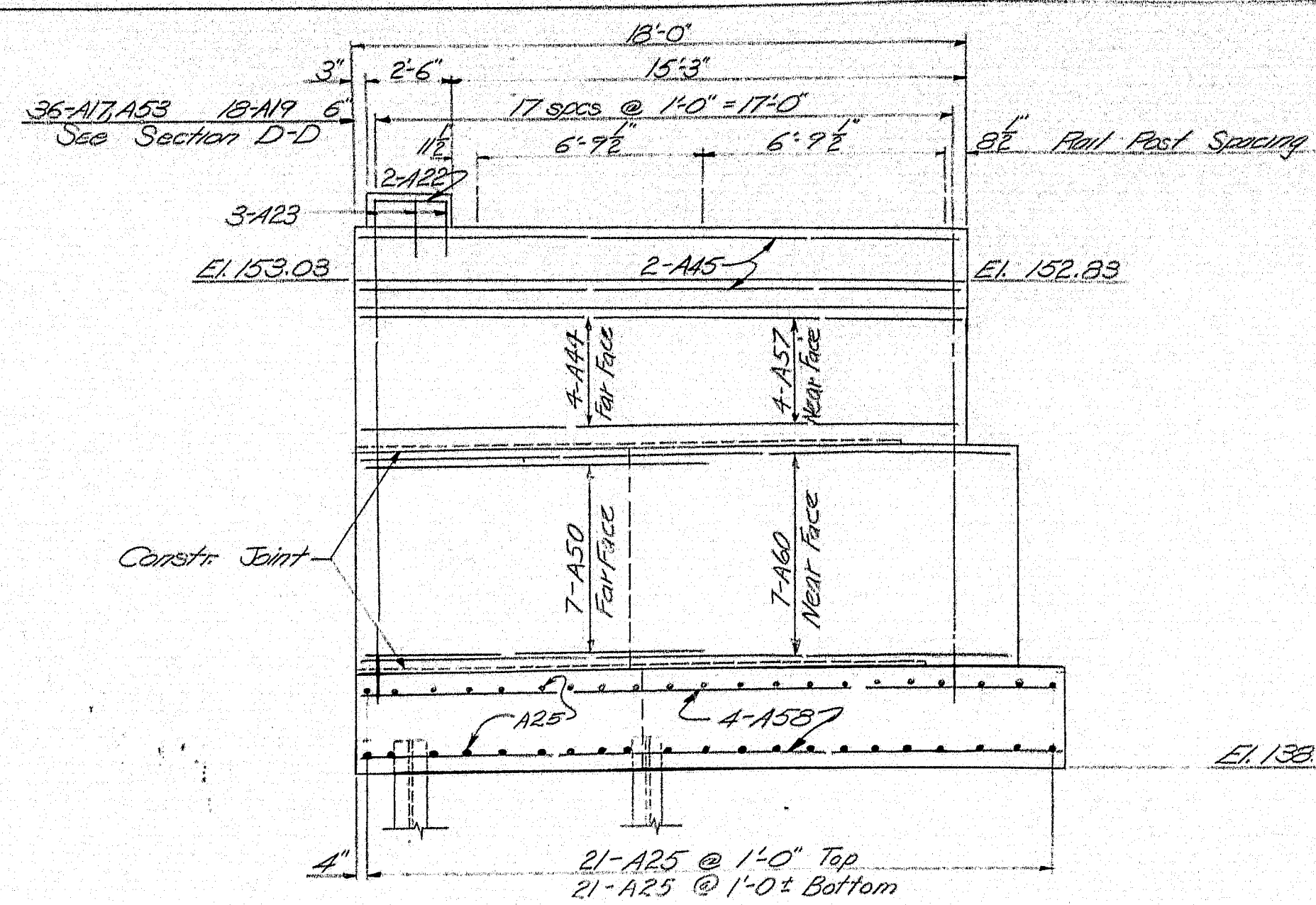
GENERAL NOTES

- Concrete in backwall 2' below finished grades at points shown.
- 2" clear to reinforcing except as noted.
- Dress shaded bearing areas on bridge seats, 1" lower all around than masonry plates and to exact elevations shown.
- Place reinforcing steel in bridge seat to clear anchor bolts.
- Coat bridge seat, face of backwall, 1' below top of slope paving on face of breastwall with Epoxy Resin Surface Sealant.
- References:
 - a. Bridge Rail ———— Sh. No. BD-102-62
 - b. Reinforcing Steel ———— Sh. No. 29
 - c. Armored Joint (See Section A-A) ———— Sh. No. 27
 - d. Fill @ Abutments ———— Sh. No. 5
 - e. Granite Curb Detail ———— Sh. No. 27
- Break bond with a suitable grade of asphalt paint at the vertical construction joint.

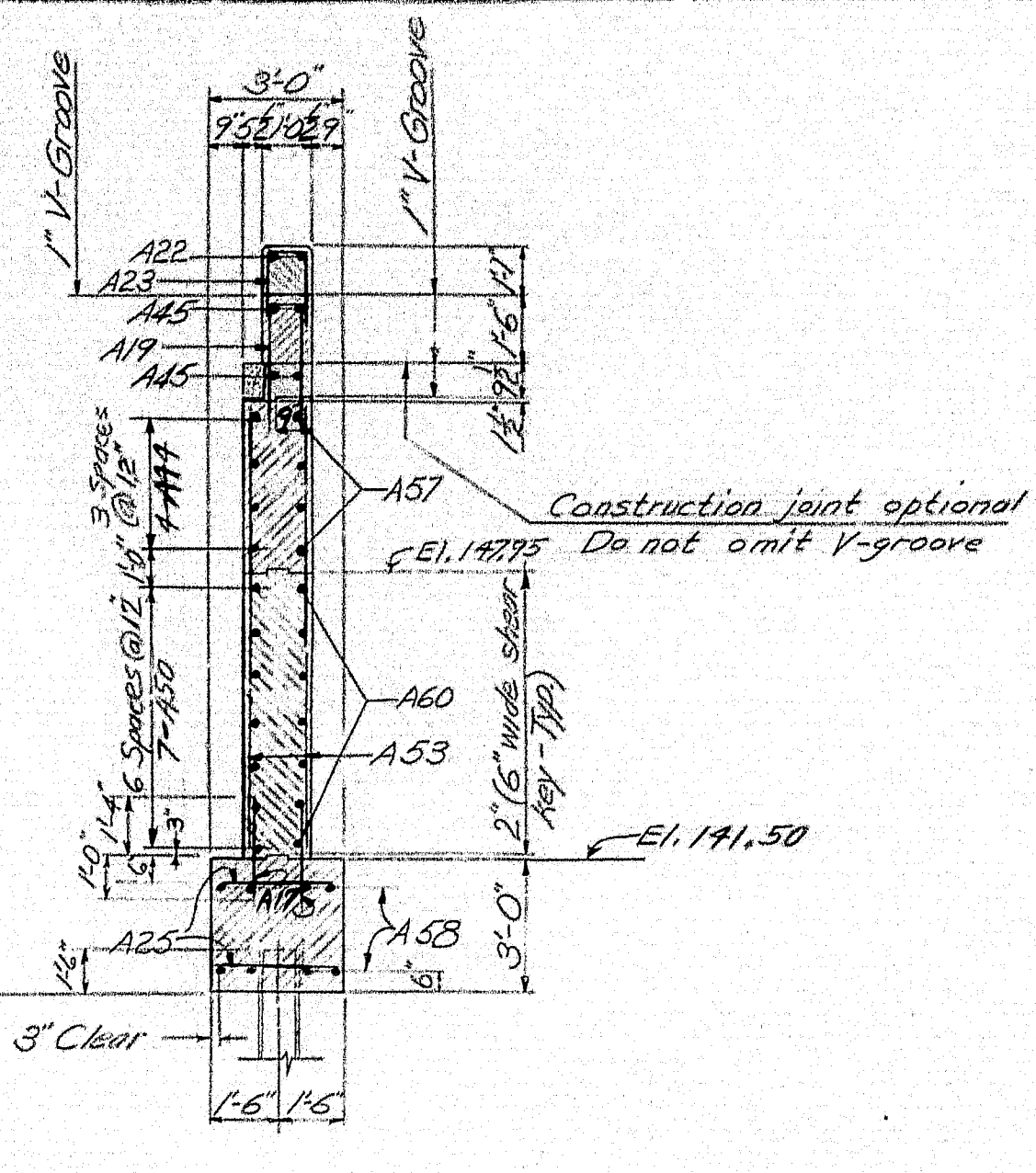
RAIL DETAIL
At Face of Backwall

STATE HIGHWAY COMMISSION
BRIDGE DIVISION
INTERSTATE 95 OVER RIVER ROAD
IN THE TOWN OF
BENTON
KENNEBEC COUNTY
ABUTMENT NO. 1 N.B.
SHEET 14 OF 29 AUGUSTA, MAINE JUNE 1963
N.B. Abutment I-95 over River Road 90-14

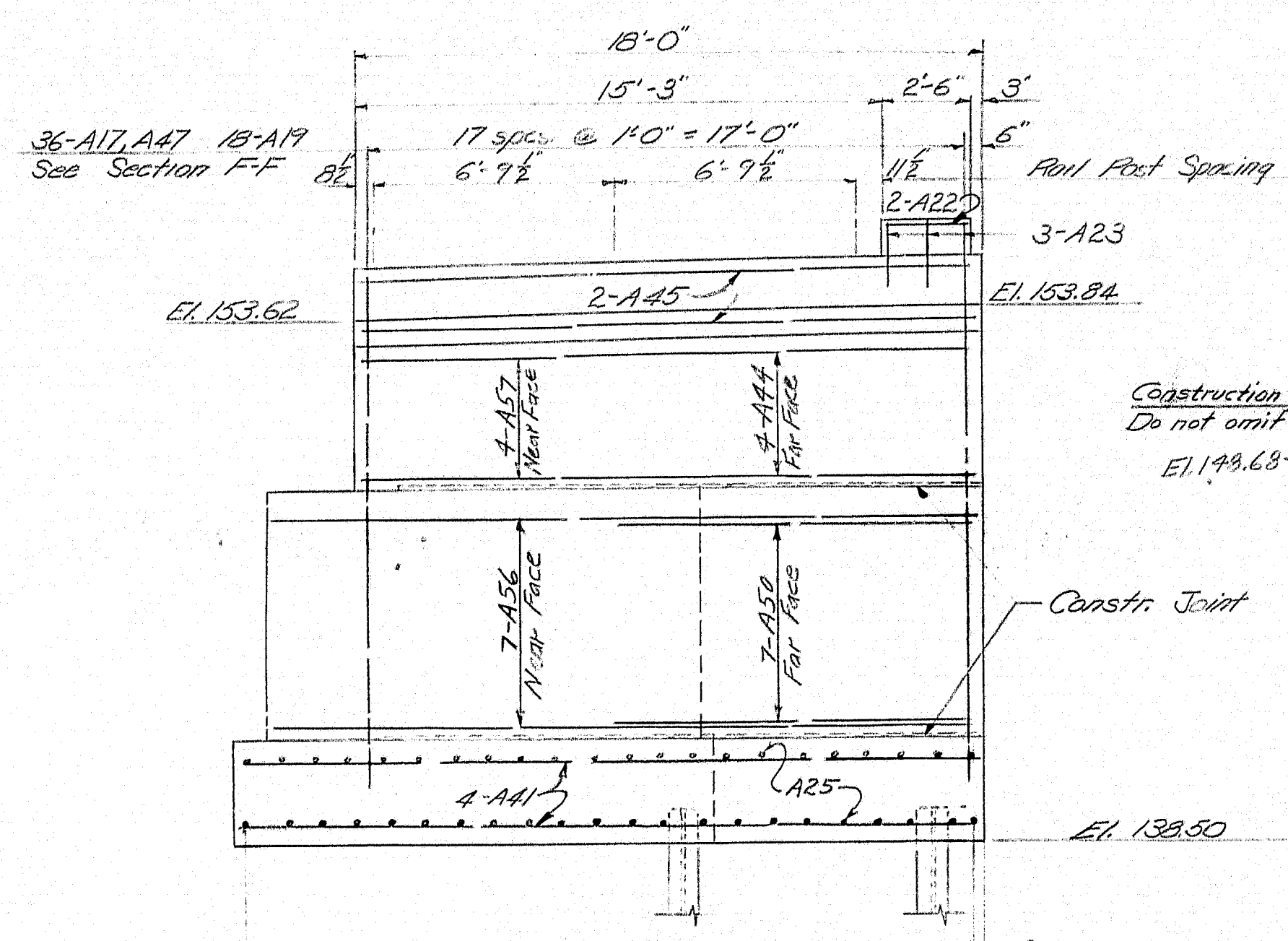




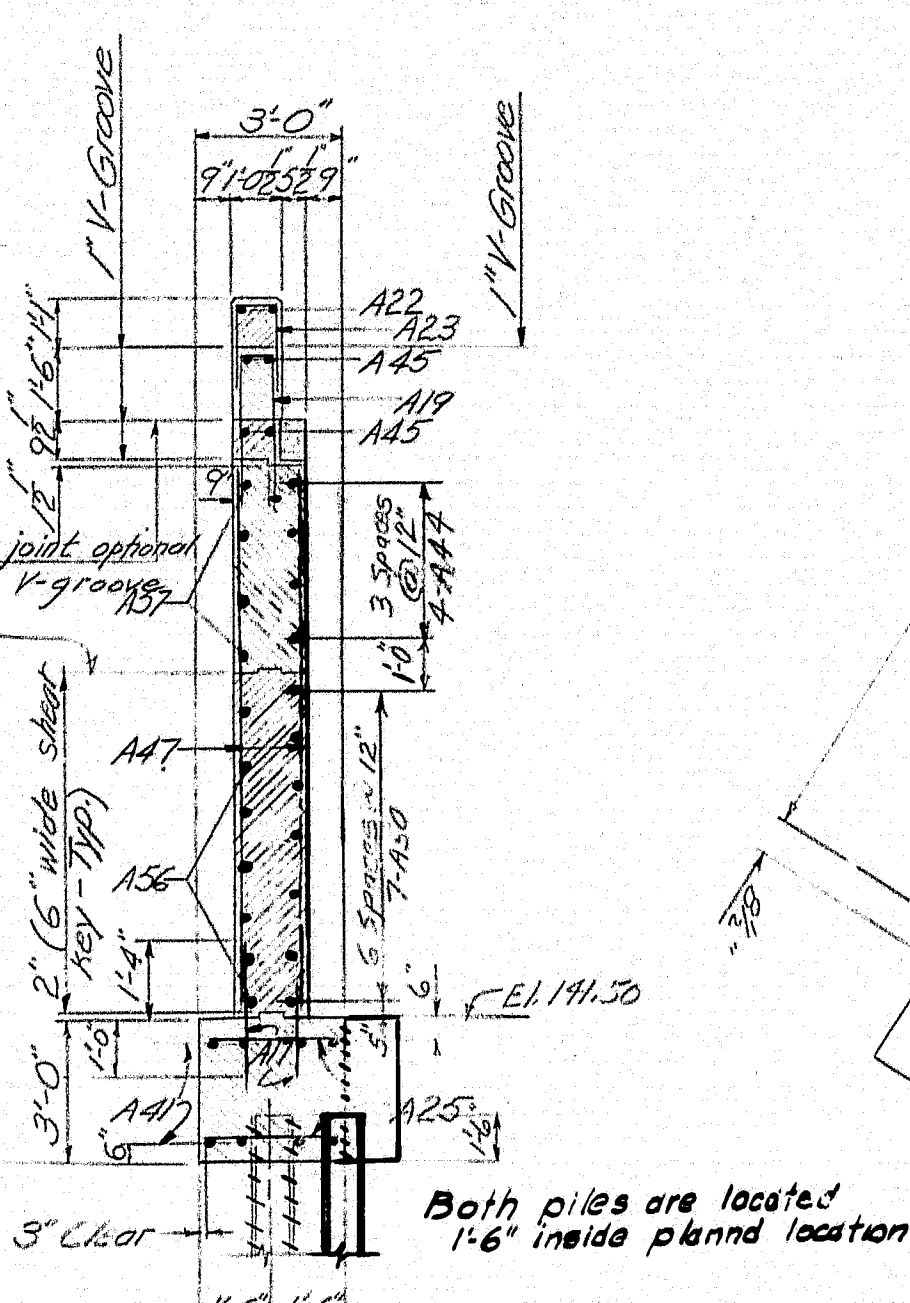
VIEW A-A



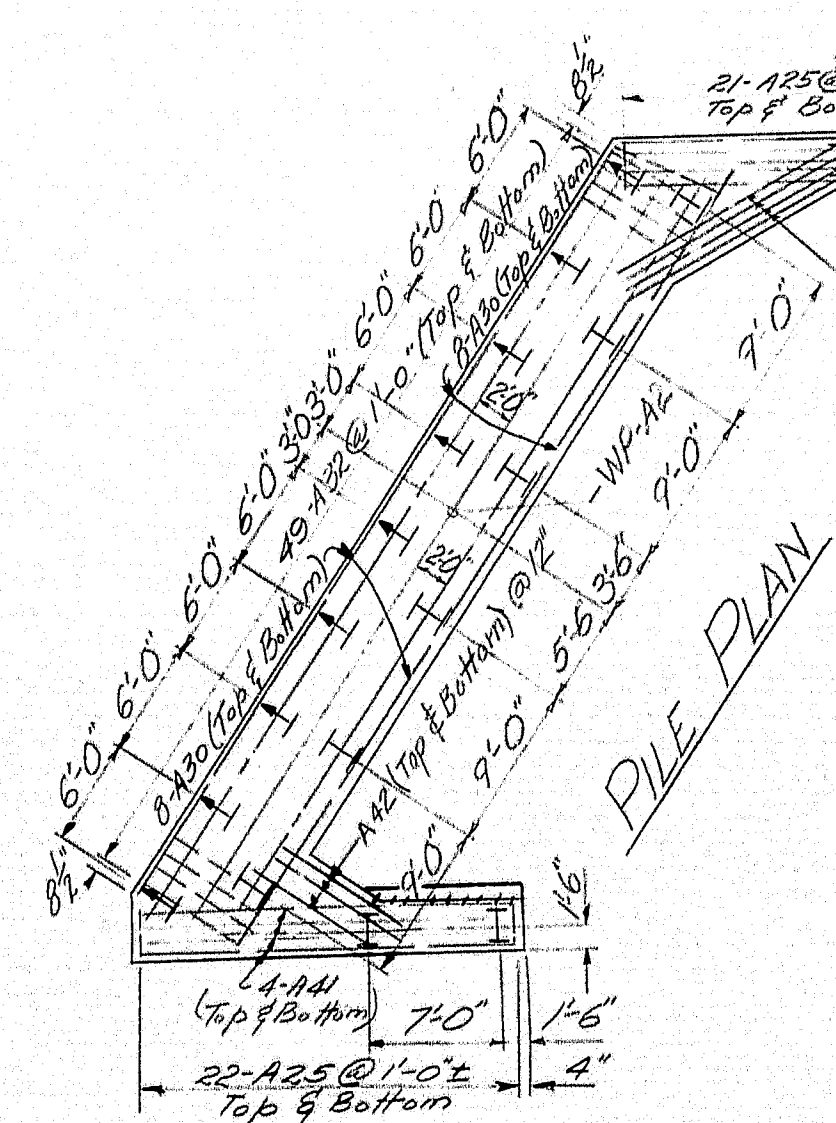
SECTION D-D



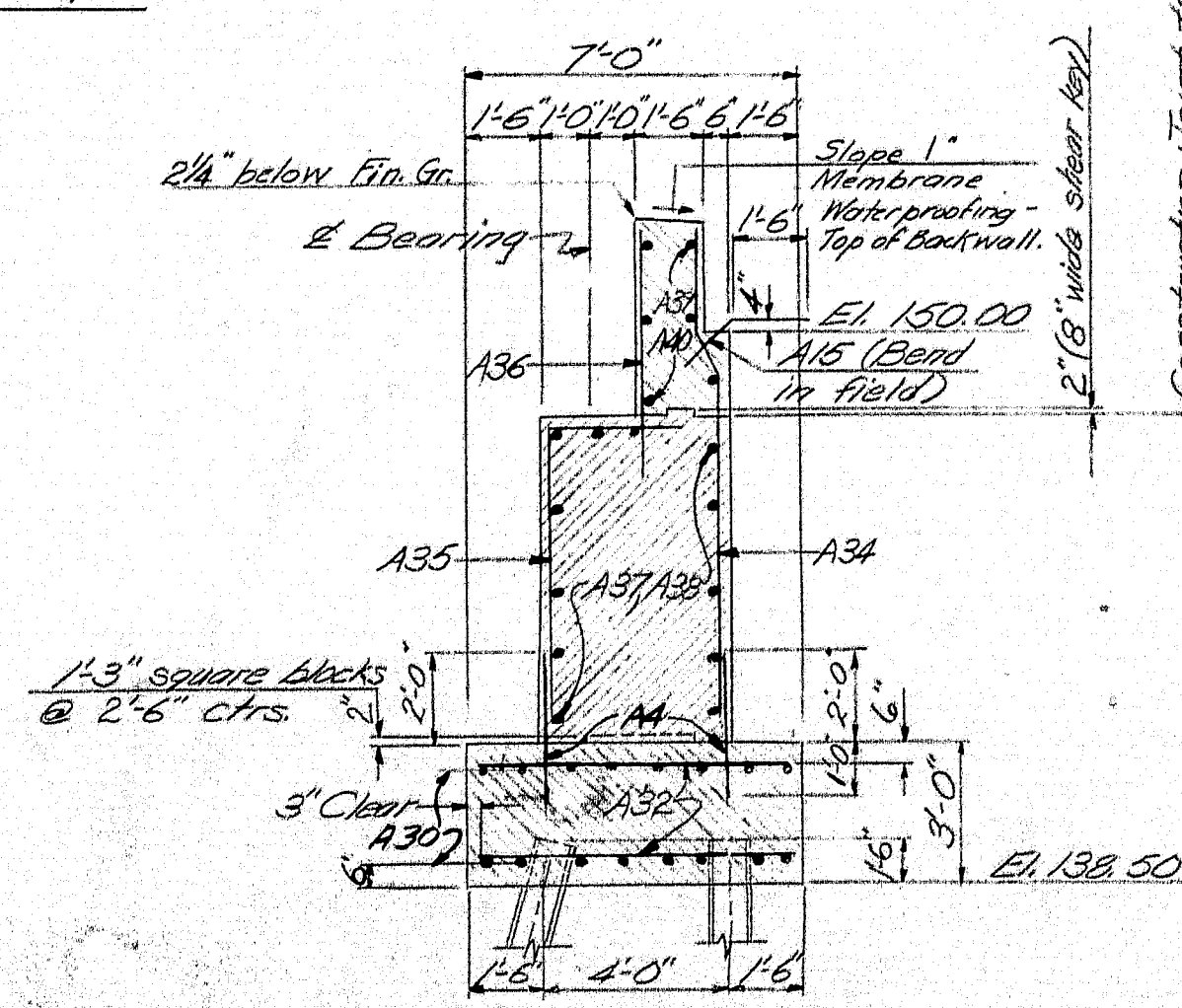
VIEW C-C



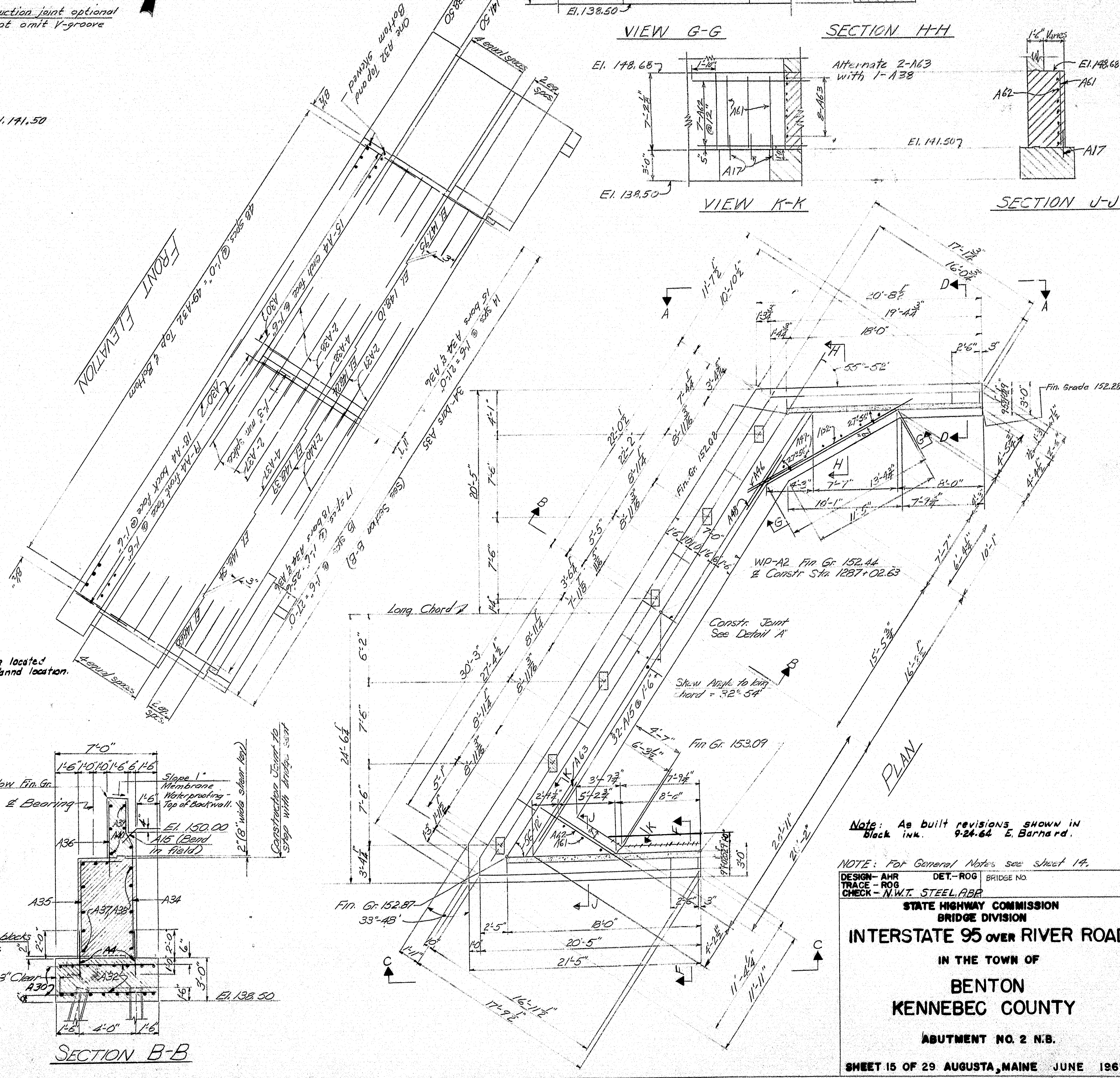
SECTION F-F



PILE NOTES
 All Piles to be 10 BP42, no pile caps required.
 Maximum Pile Load = 37 tons per pile.
 Batter piles indicated thus, 3' per foot in direction of arrow. All others plumb.
 Drive piles to ledge or practical refusal.
 19 piles required; Estimated Length 25'-0"



SECTION B-B



NOTE: As built revisions shown in black ink. 9-24-64 E. Barnard.

NOTE: For General Notes see sheet 14.

DESIGN - AHR
 TRACE - ROG
 CHECK - N.W.T. STEEL ABB

DET - ROG
 BRIDGE NO.

**STATE HIGHWAY COMMISSION
 BRIDGE DIVISION**

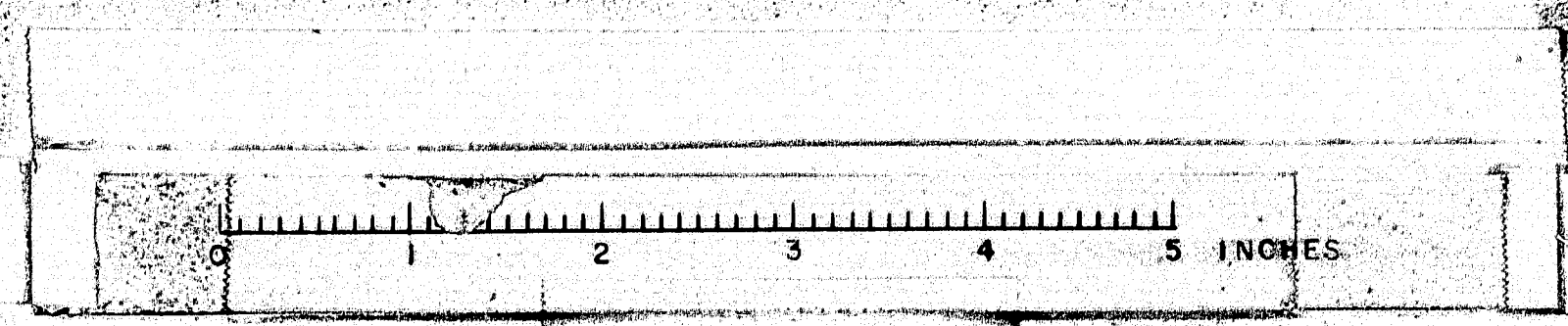
INTERSTATE 95 OVER RIVER ROAD

**IN THE TOWN OF
 BENTON
 KENNEBEC COUNTY**

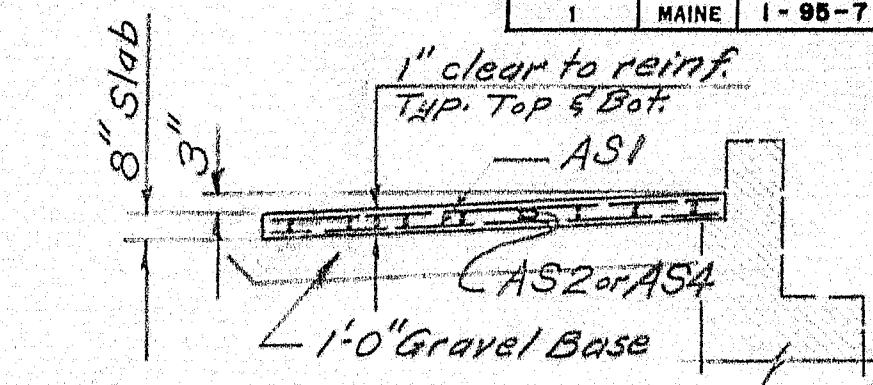
ABUTMENT NO. 2 N.B.

SHEET 15 OF 29 AUGUSTA, MAINE JUNE 1963

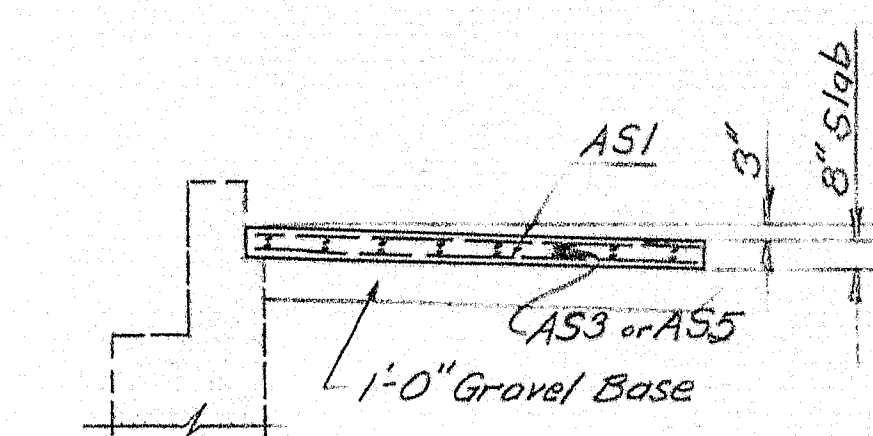
90-15



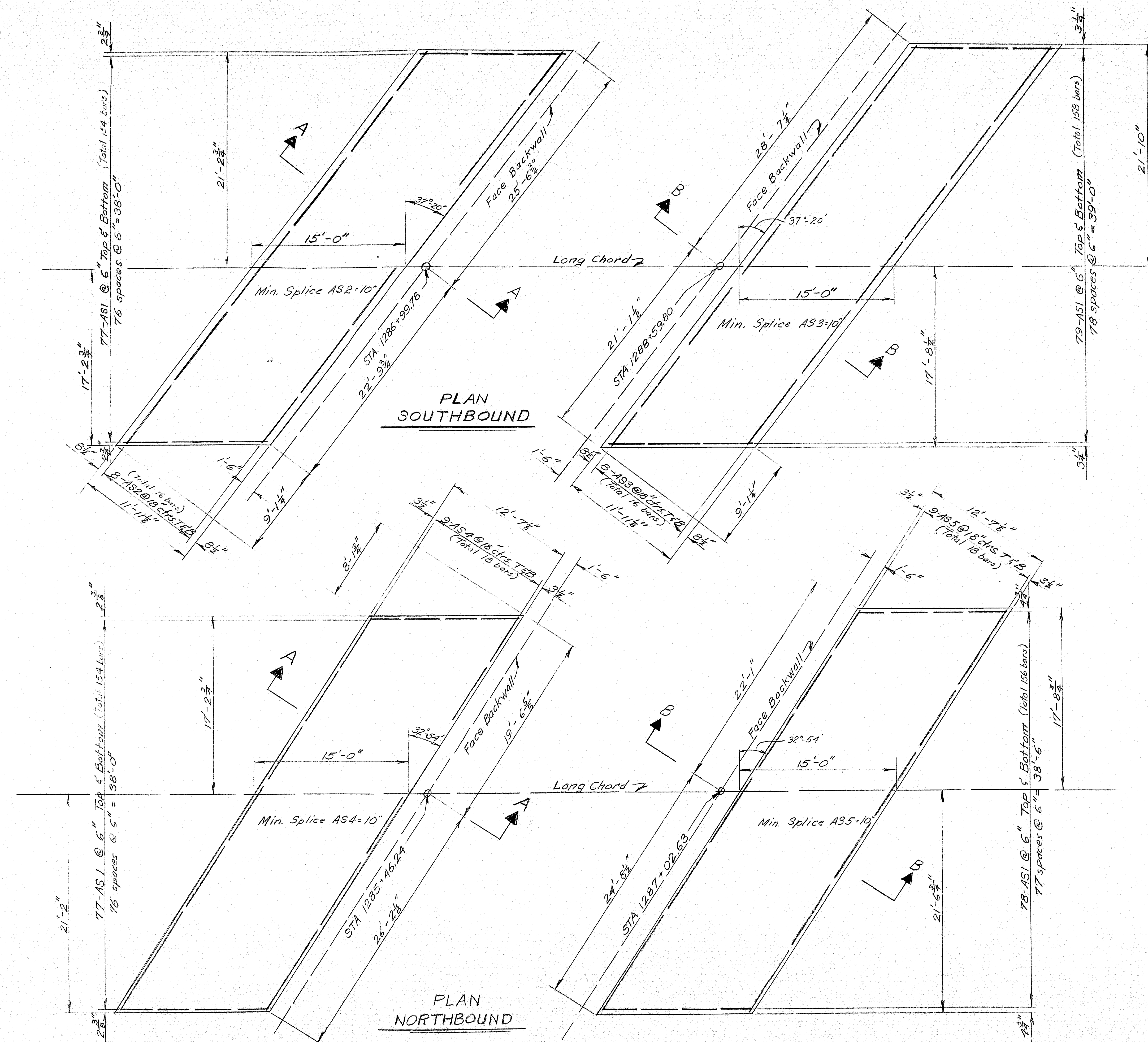
B.P. R. REV. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-7(37)	16	29



SECTION A-A



SECTION B-B

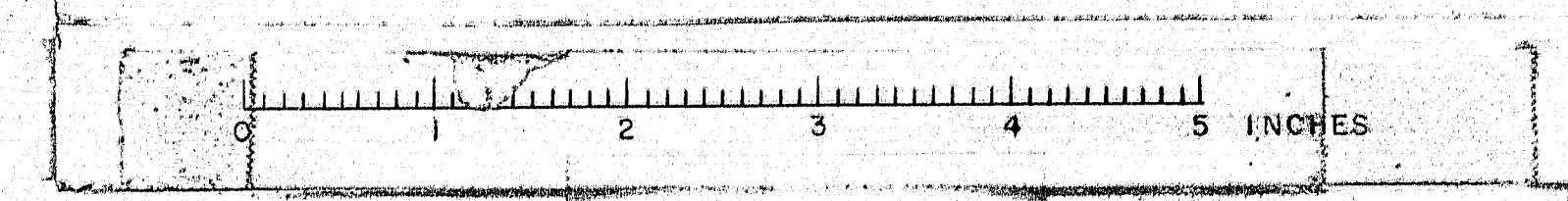


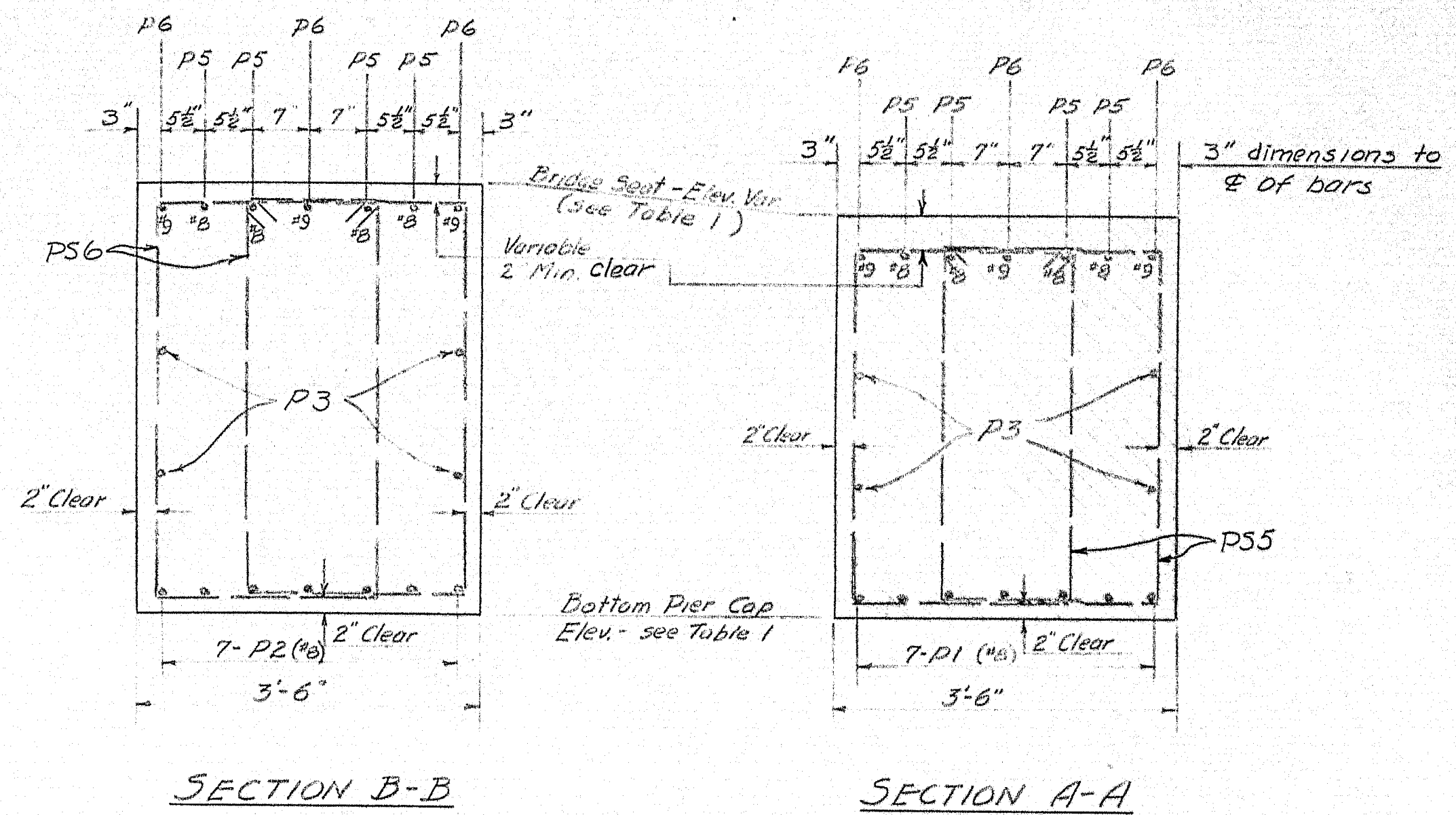
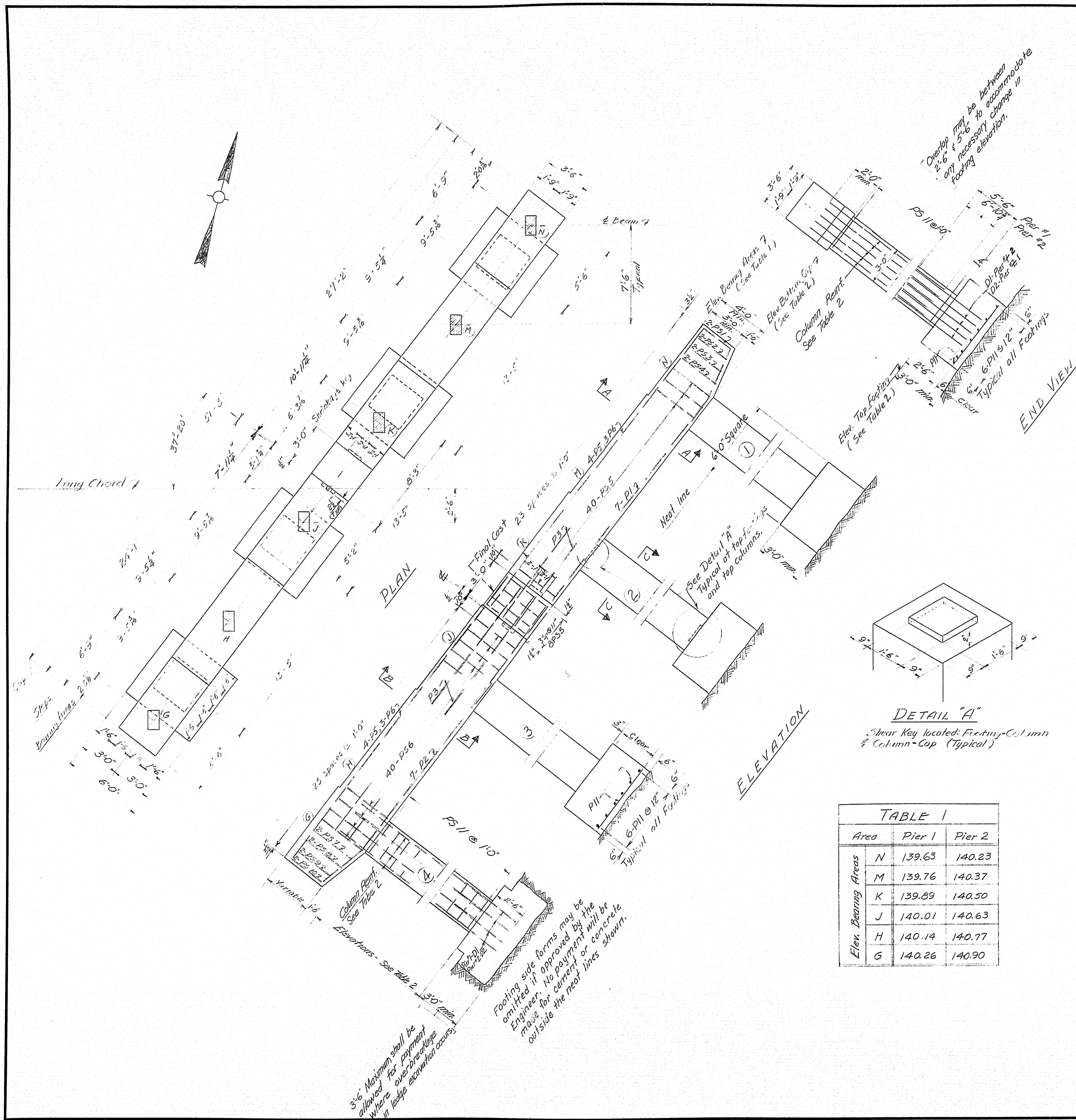
NOTE:
Payment for concrete in approach slabs to be made under Item 73-23 Portland Cement Concrete Retaining Walls and Retaining Walls.

DESIGN - AMR TRACE - RVL CHECK - A.B.P.	DET - RVL SURVEY - PLOT -	BRIDGE NO. STATE HIGHWAY COMMISSION BRIDGE DIVISION INTERSTATE 95 OVER RIVER ROAD IN THE TOWN OF BENTON KENNEBEC COUNTY APPROACH SLABS SB & NB
---	---------------------------------	---

SHEET 16 OF 29 AUGUSTA, MAINE JUNE 1963

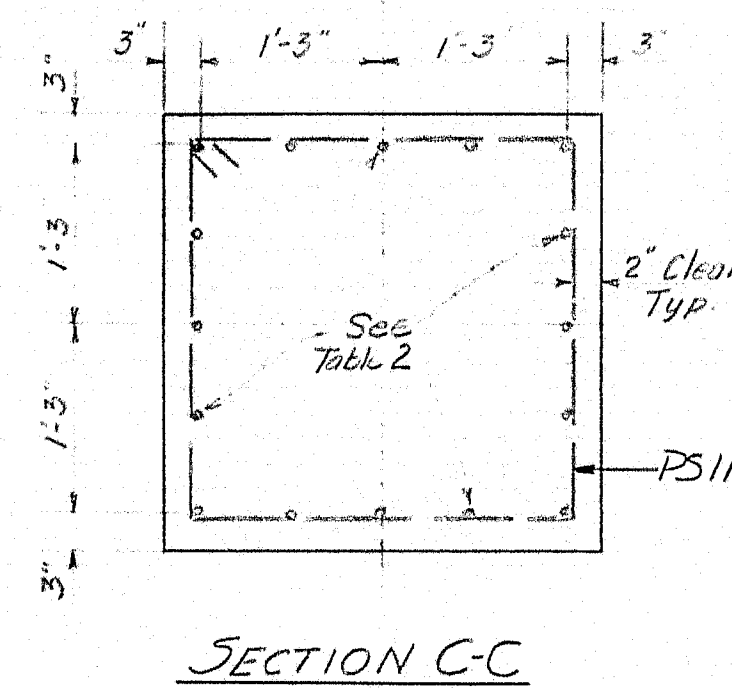
SB & NB. APPROACH SLABS I-95 RIVER RD. 90-16



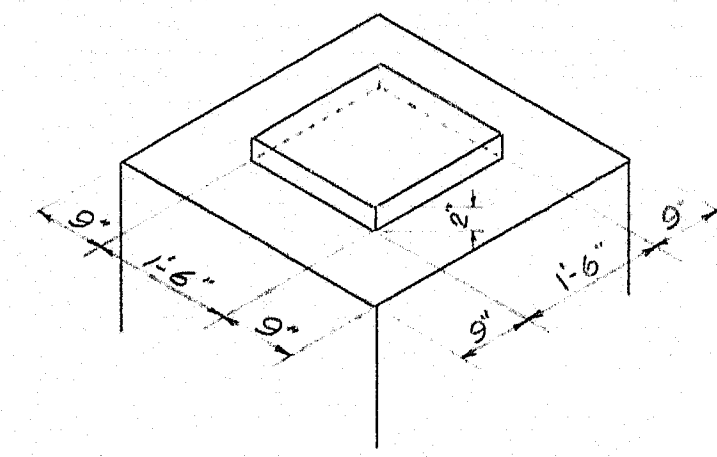


	Column Location	Column Reinf.	Column Stirrups	Top of Column El.	Top of Footing El.	Column Height	Approx Ledge El.
PIER 1	1	16-P22 ¹	15-P311	135.63	135.92	14'-7 1/2"	119.00
	2	16-P25 ¹	15-P311	135.63	136.98	15'-7 1/2"	117.00
	3	16-P24 ¹	18-P311	135.63	118.90	17'-7 1/2"	115.00
	4	16-P25 ¹	17-P311	135.63	117.90	16'-7 1/2"	115.00
PIER 2	1, 2, 3, 4	16-P26	18-P311	136.23	117.90	21'-2 1/2"	112.00
	2	16-P26	18-P311	136.23	119.00	17.23	
	3	16-P26	20-P311	136.23	117.00	18.23	
	4	16-P26	19-P311	136.23	117.50	18.73	

Note: For ledge elevations see Field Book 2, pages 49, 51.



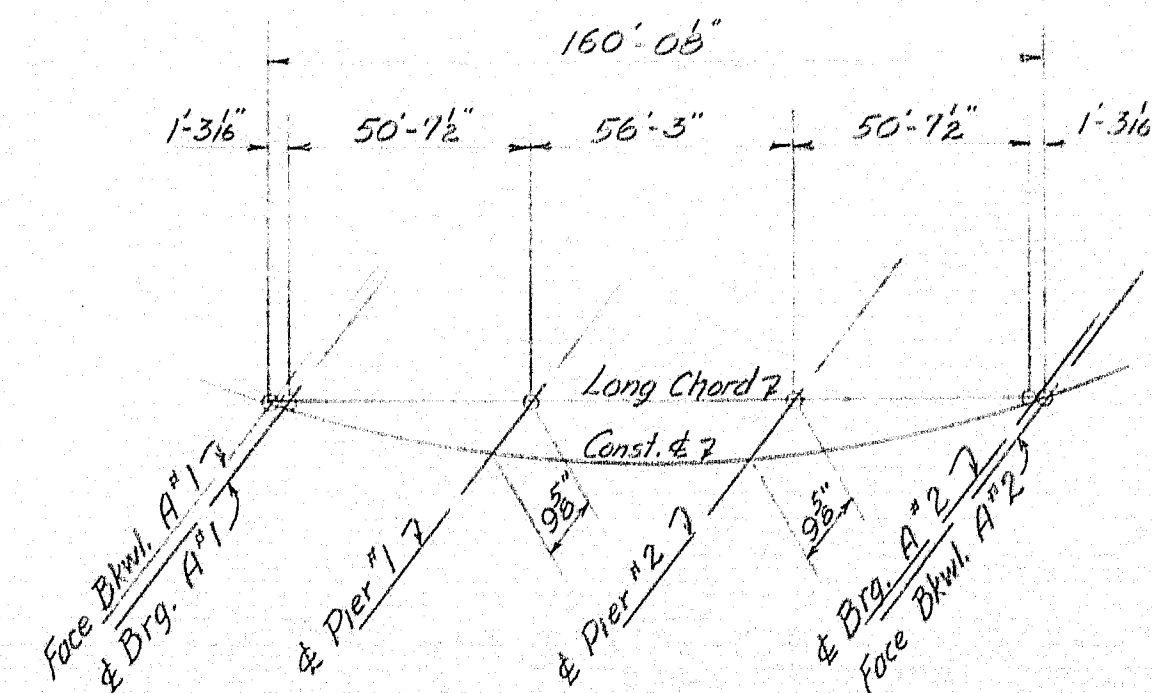
SECTION C-C



DETAIL "A"

Shear Key located: Footing-Column
& Column-Cap (Typical)

Area	Pier 1	Pier 2
Elev. Bearing Areas	N 139.63	140.23
	M 139.76	140.37
	K 139.89	140.50
	J 140.01	140.63
	H 140.14	140.77
	G 140.26	140.90



LONG CHORD LAYOUT

Note: As built revisions shown in black ink. 9-24-64 E. Barnard

NOTES

Dress shod bearing areas to exact elevations shown and 1" larger all around than the required masonry plate.

Reinforcing steel in top of cap to clear anchor bolts.

All weathered or broken ledge shall be removed before any footing concrete is placed.

Maximum footing Pressure = 14 Tons/sq. ft.
Top of footing elevations may be changed

by the Engineer in the field to fit local conditions.

The top of any footing shall not be less than 1'-0" below the surface of the final

Soils information indicates that some water should be anticipated in the

should be anticipated in pier foundation excavations. No direct payment will be made for unanticipated items.

for unwatering foundations. This work should be considered as incidental to the contract items.

items.

DESIGN - HBD	DET - EBF	BRIDGE NO
--------------	-----------	-----------

DESIGN-HRD	DET.-FBF	BRIDGE NO.
TRACE- FBF		SURVEY-
CHECK- T,H,K.		PLOT-

**STATE HIGHWAY COMMISSION
BRIDGE DIVISION**

INTERSTATE 95 OVER RIVER ROAD

IN THE TOWN OF

BENTON

KENNEBEC COUNTY

PIERS S.B.

SHEET 17 OF 29 AUGUSTA, MAINE JUNE 1963

PIERS S.B. 90-17

100

100

[illegible]

1985

1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100	101	102	103	104	105	106	107	108	109	110	111	112	113	114	115	116	117	118	119	120	121	122	123	124	125	126	127	128	129	130	131	132	133	134	135	136	137	138	139	140	141	142	143	144	145	146	147	148	149	150	151	152	153	154	155	156	157	158	159	160	161	162	163	164	165	166	167	168	169	170	171	172	173	174	175	176	177	178	179	180	181	182	183	184	185	186	187	188	189	190	191	192	193	194	195	196	197	198	199	200	201	202	203	204	205	206	207	208	209	210	211	212	213	214	215	216	217	218	219	220	221	222	223	224	225	226	227	228	229	230	231	232	233	234	235	236	237	238	239	240	241	242	243	244	245	246	247	248	249	250	251	252	253	254	255	256	257	258	259	260	261	262	263	264	265	266	267	268	269	270	271	272	273	274	275	276	277	278	279	280	281	282	283	284	285	286	287	288	289	290	291	292	293	294	295	296	297	298	299	300	301	302	303	304	305	306	307	308	309	310	311	312	313	314	315	316	317	318	319	320	321	322	323	324	325	326	327	328	329	330	331	332	333	334	335	336	337	338	339	340	341	342	343	344	345	346	347	348	349	350	351	352	353	354	355	356	357	358	359	360	361	362	363	364	365	366	367	368	369	370	371	372	373	374	375	376	377	378	379	380	381	382	383	384	385	386	387	388	389	390	391	392	393	394	395	396	397	398	399	400	401	402	403	404	405	406	407	408	409	410	411	412	413	414	415	416	417	418	419	420	421	422	423	424	425	426	427	428	429	430	431	432	433	434	435	436	437	438	439	440	441	442	443	444	445	446	447	448	449	450	451	452	453	454	455	456	457	458	459	460	461	462	463	464	465	466
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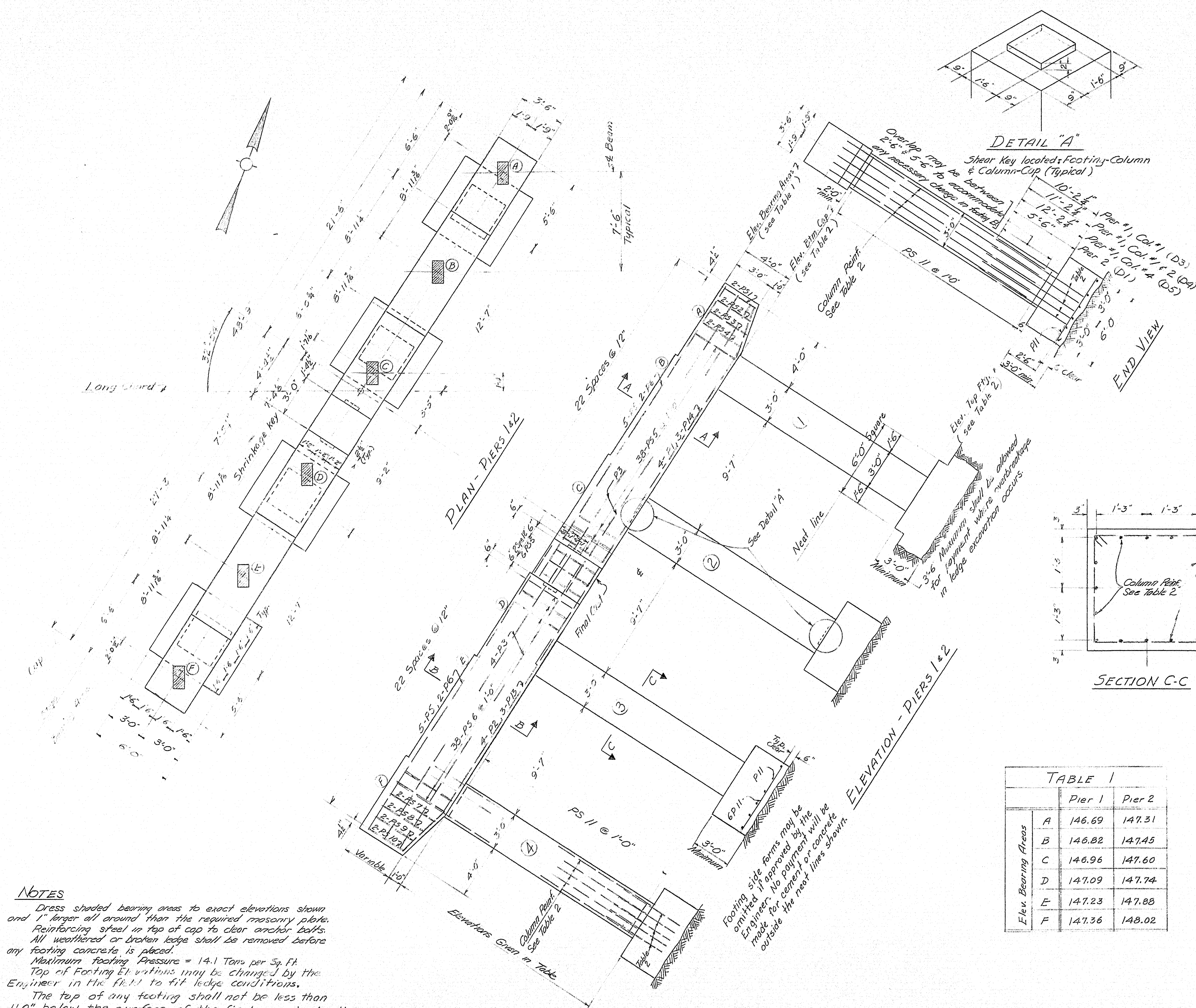


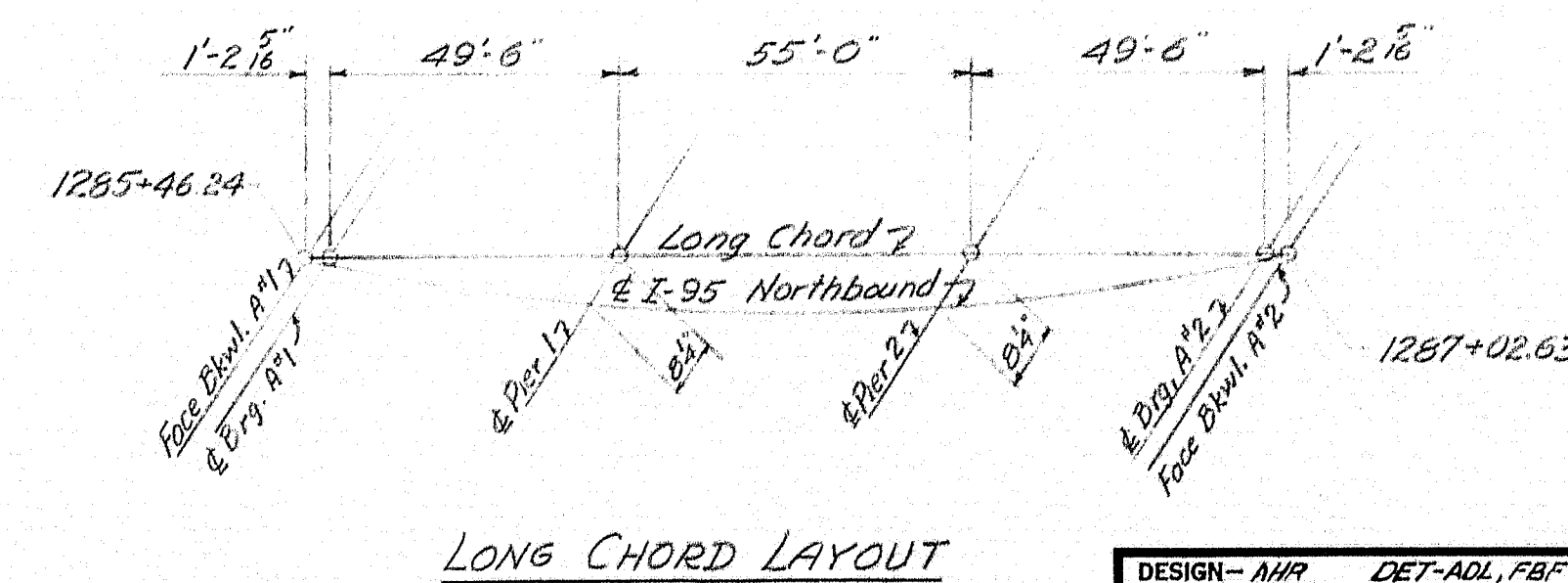
TABLE 2

Column Location	Column Reinf.	Column Stirrups	Top of Column Elev.	Top of Footing Elev.	Column Height	Approx. Ledge Elev.	Footing Details
PIER 1	1	16-P15	25-PS11	142.69	127.00	24'-8"	16-D3
	2	16-P15	26-PS11	142.69	127.00	25'-8"	16-D4
	3	16-P15	26-PS11	142.69	127.00	25'-8"	16-D4
	4	16-P15	27-PS11	142.69	127.00	26'-8"	16-D5
PIER 2	1	16-P18	17-PS11	143.31	126.00	17'-3"	16-D1
	2	16-P19	17-PS11	143.31	126.50	16'-9"	16-D1
	3	16-P20	15-PS11	143.31	127.00	16'-3"	16-D1
	4	16-P21	15-PS11	143.31	128.00	15'-3"	16-D1

Note: For ledge elevations see Field Book 2, pages 48, 50.

TABLE 1

	Pier 1	Pier 2
A	146.69	147.31
B	146.82	147.45
C	146.96	147.60
D	147.09	147.74
E	147.23	147.88
F	147.36	148.02



NOTES

Dress shaded bearing areas to exact elevations shown and 1" larger all around than the required masonry plate. Reinforcing steel in top of cap to clear anchor bolts. All weathered or broken ledge shall be removed before any footing concrete is placed. Maximum footing pressure = 14.1 Tons per Sq. Ft. Top of footing elevations may be changed by the Engineer in the field to fit ledge conditions. The top of any footing shall not be less than 1'-0" below the surface of the final ground elevation.

Note: As built revisions shown in black ink. 9-24-64 E. Barnard

DESIGN - MRF
TRACE - FBF
CHECK - T.H.K.

DET-ADL, FBF
SURVEY - FLOT

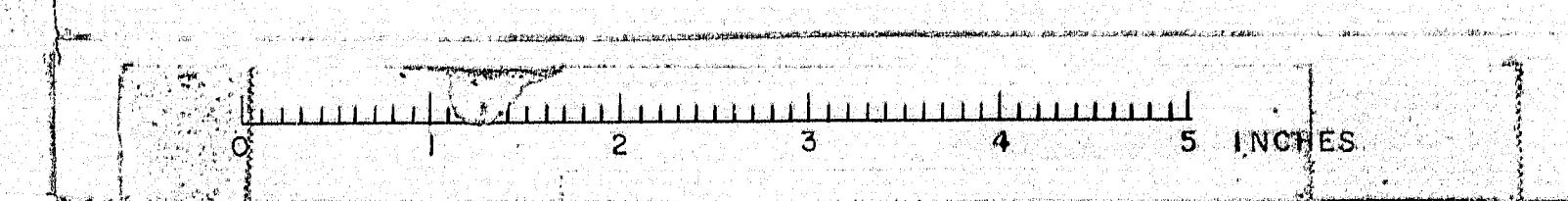
BRIDGE NO. 1-95-7 (37)

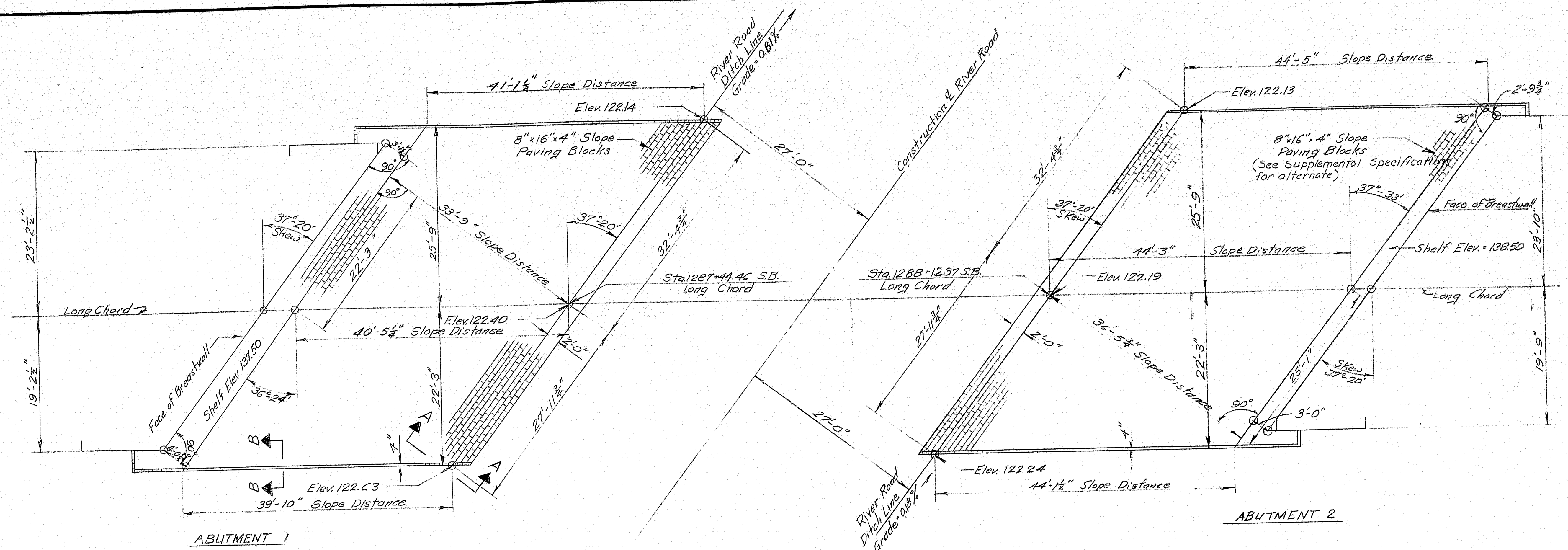
STATE HIGHWAY COMMISSION
BRIDGE DIVISION

INTERSTATE 95 OVER RIVER ROAD
IN THE TOWN OF
BENTON
KENNEBEC COUNTY

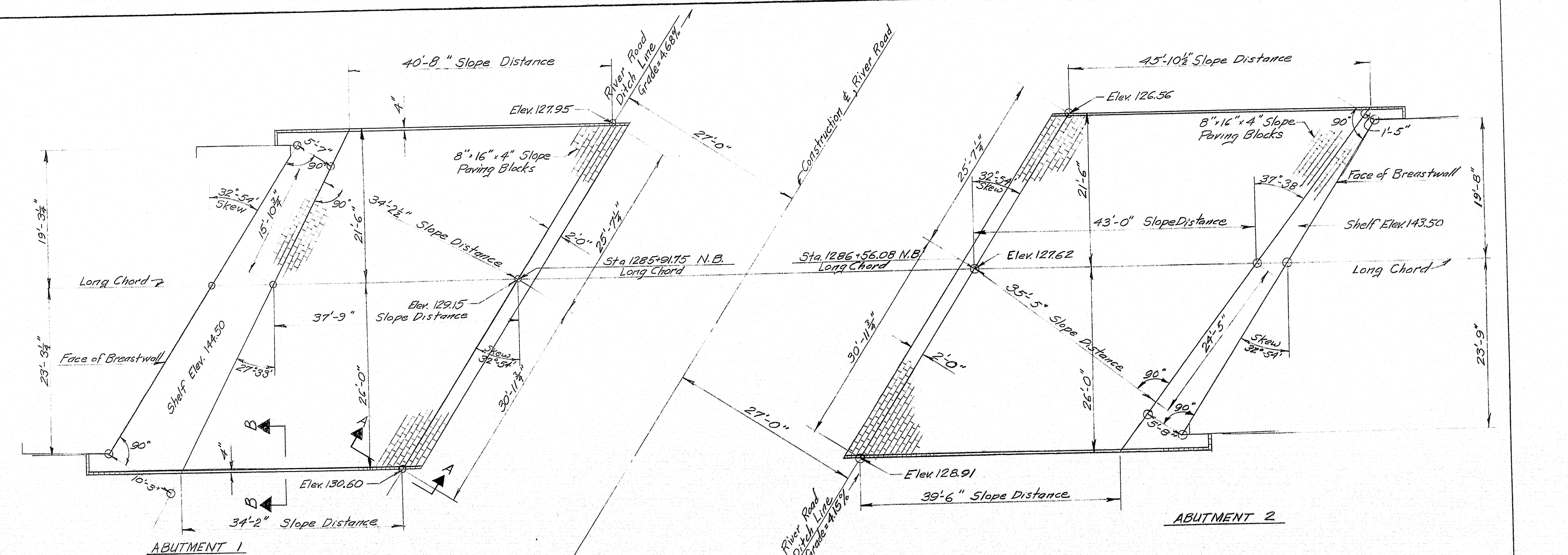
PIERS N.B.

SHEET 18 OF 29 AUGUSTA, MAINE JUNE 1963

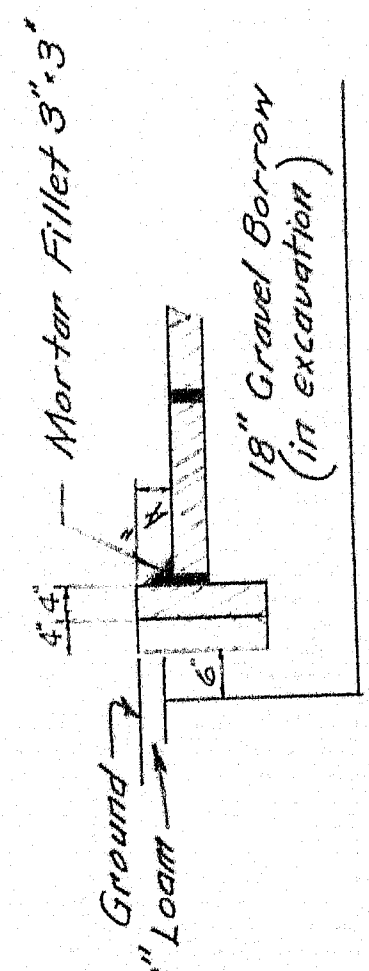
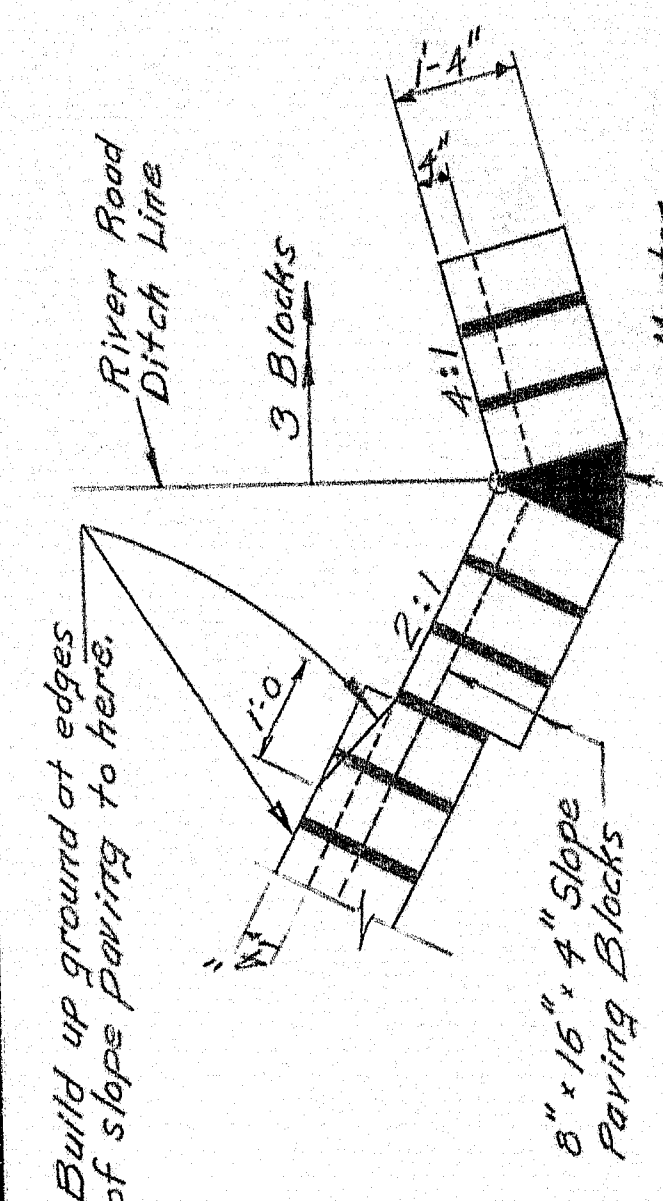




PLAN - SOUTHBOUND



PLAN - NORTHBOUND

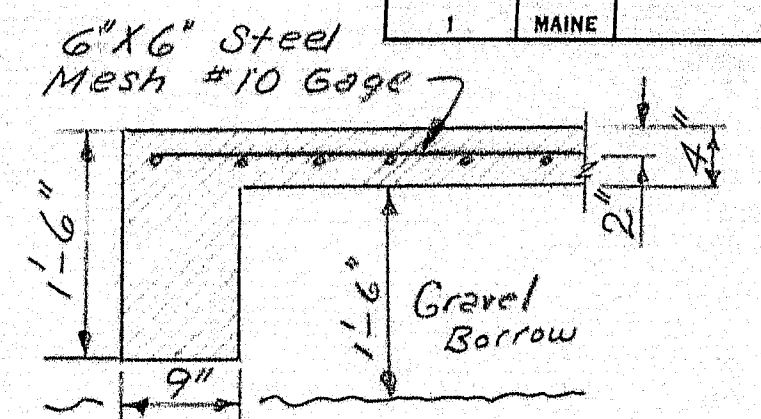
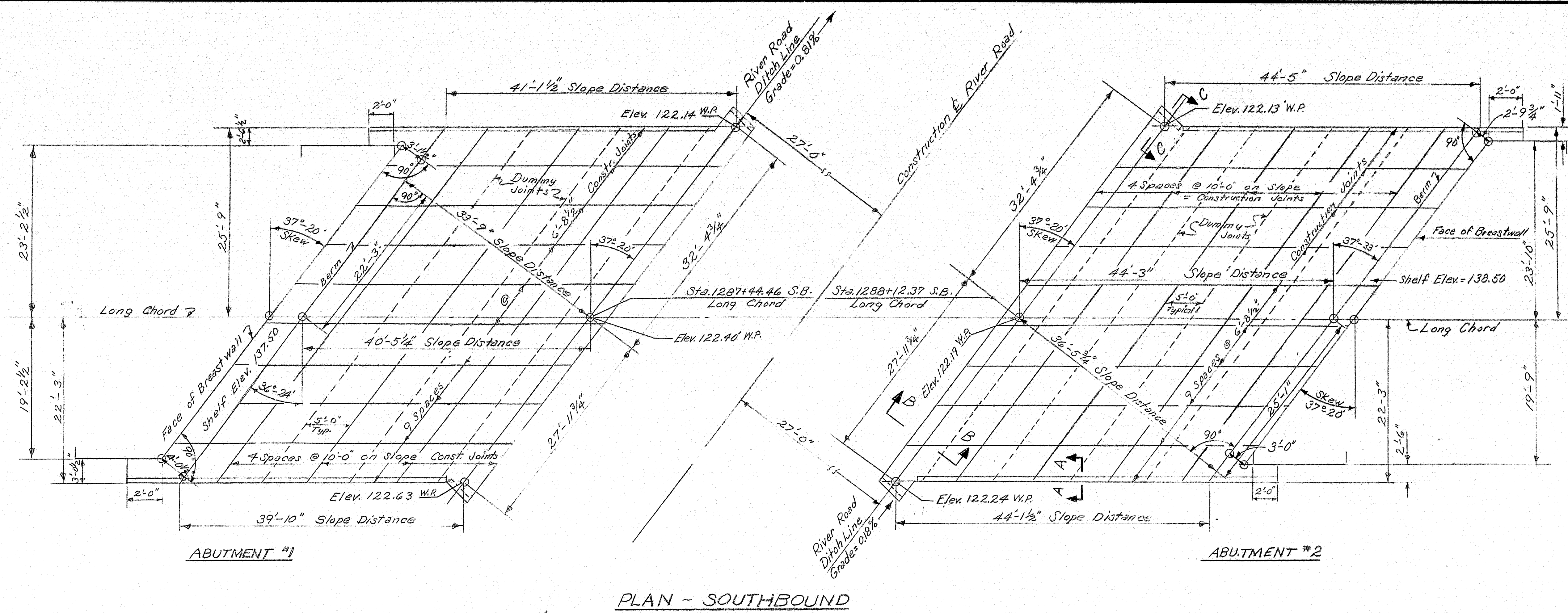


NOTES:

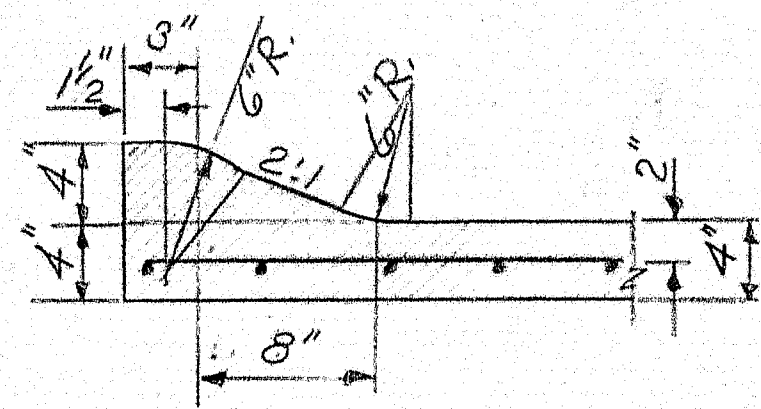
- Excavation for 18" Gravel Borrow (excavated areas) under Slope paving to be paid for under Item 204-14 Structural Earth Excavation, Piers
- Depth of Gravel Borrow under slope paving may be reduced or omitted if in the opinion of the Engineer the existing material is suitable.

DESIGN - A.H.R. H.R.D. DET. 20	BRIDGE NO. 19
TRACE - R.W.L.	SURVEY - 19
CHECK - D. B. B. B.	PLOT - 19
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95 OVER RIVER ROAD	
IN THE TOWN OF BENTON KENNEBEC COUNTY	
SLOPE PAVING DETAILS SB & NB	
SHEET 19 OF 29 AUGUSTA, MAINE JUNE 1963	

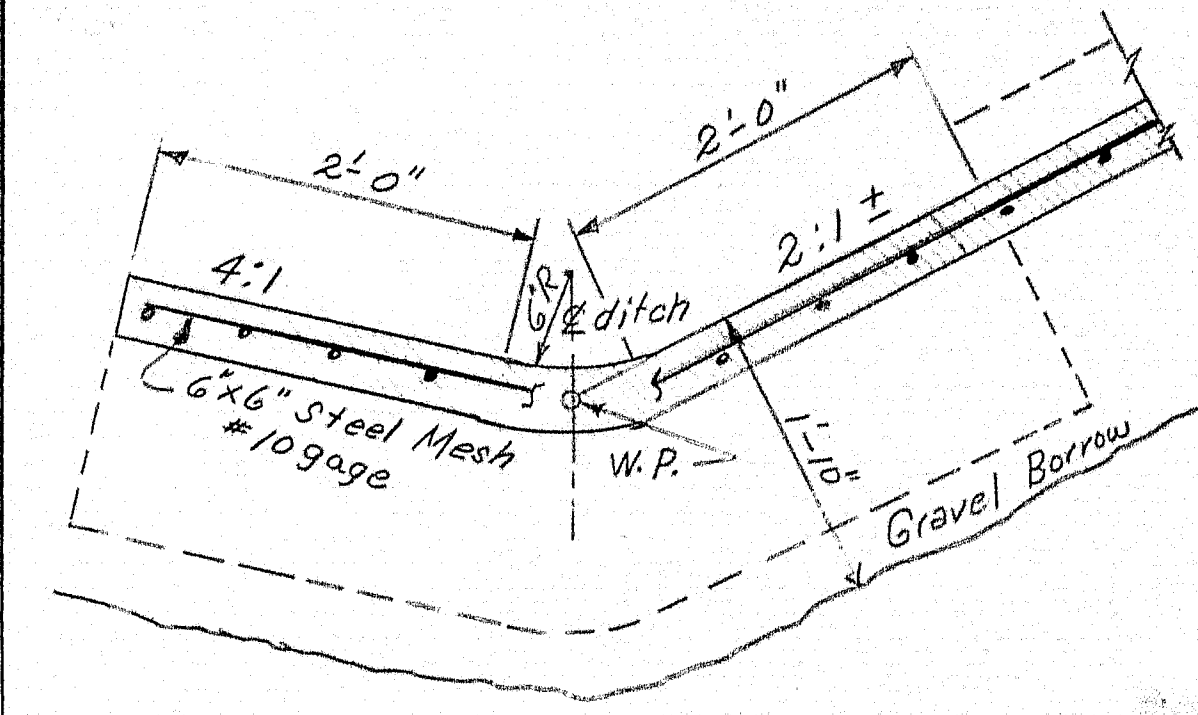
B. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE			



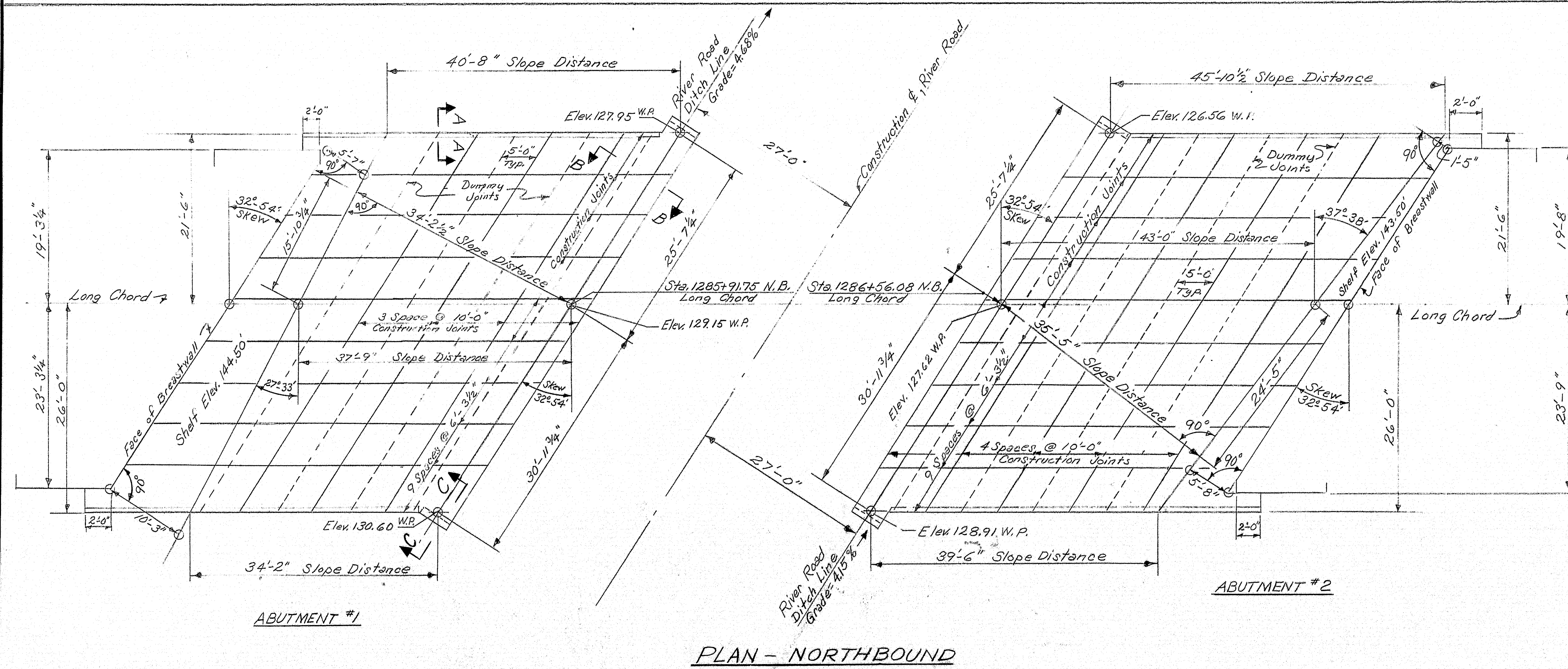
SECTION "C-C" (TYPICAL)



SECTION "A-A" (TYPICAL)



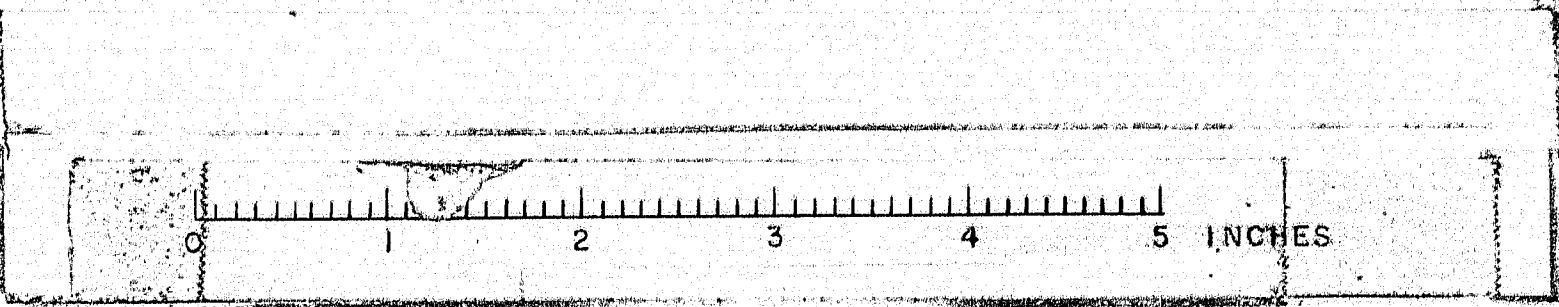
SECTION "B-B" (TYPICAL)

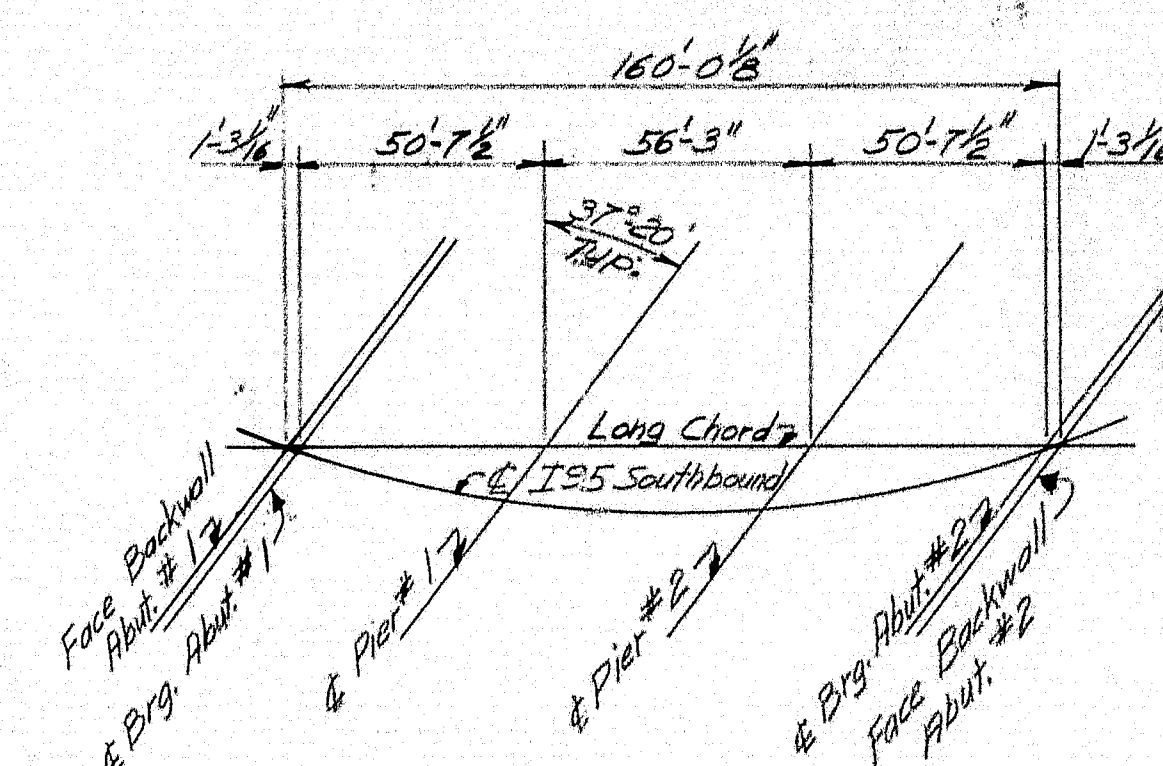
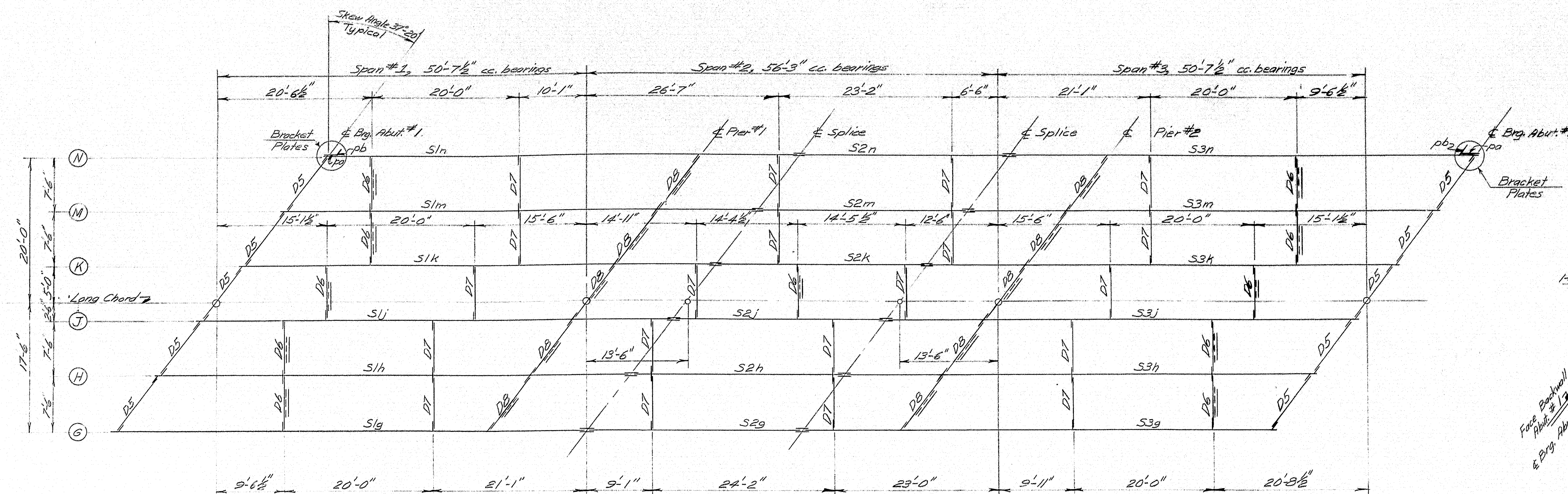


- NOTES:
1. Break bond at Construction Joints with a coat of asphalt paint.
 2. Reinforce with #10 Gage 6"x6" Steel Mesh, not to pass through Construction Joints.
 3. At Contractors option, sections of this same strip may be cast in order. Bond will be broken between adjoining sections with 1/4" of Preformed Expansion Joint Filler.
 4. Dummy Joints shall be made with a side walk edging tool to a depth of 1/4".
 5. The 18" Gravel Borrow under this slope paving may be reduced or omitted, if in the opinion of the Engineer, the existing material is suitable.
 6. Payment for excavation for Gravel Borrow under Slope Paving to be made under Item 204-14, Structural Earth Excavation - Piers.

DESIGN - AHR, CWM	BRIDGE NO.
TRACE -	SURVEY -
CHECK - CDH	PLOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION	
INTERSTATE 95 OVER RIVER ROAD	
IN THE TOWN OF	
BENTON	
KENNEBEC COUNTY	
SLOPE PAVING DETAILS S.B. & N.B.	
SHEET 19A OF 29 AUGUSTA, MAINE NOV. 1963	

90-19A



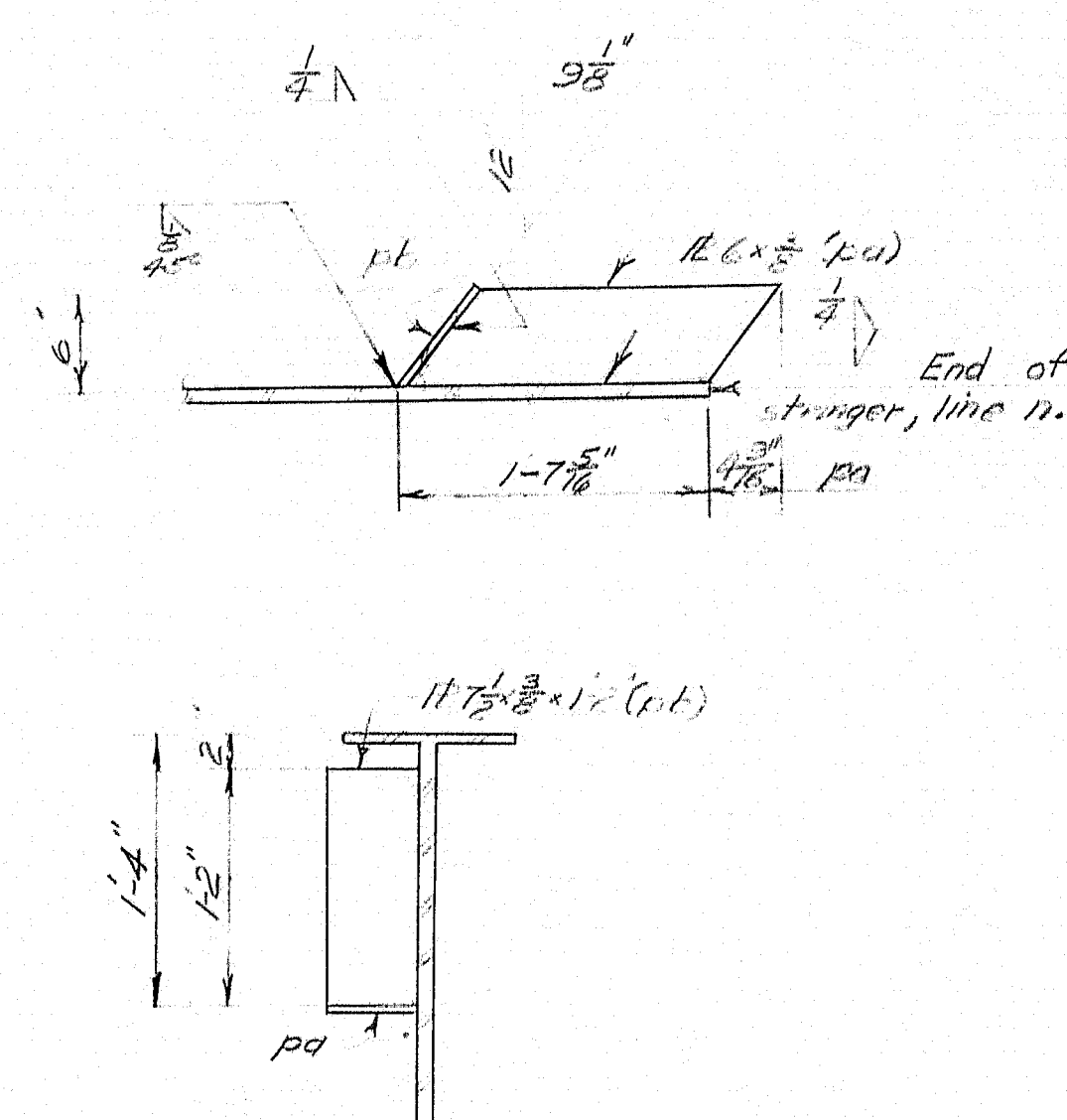


LONG CHORD LAYOUT

Stringers:
 S1 33 WF 130 6 Req'd.
 S2 33 WF 118 6 Req'd.
 S3 33 WF 130 6 Req'd.

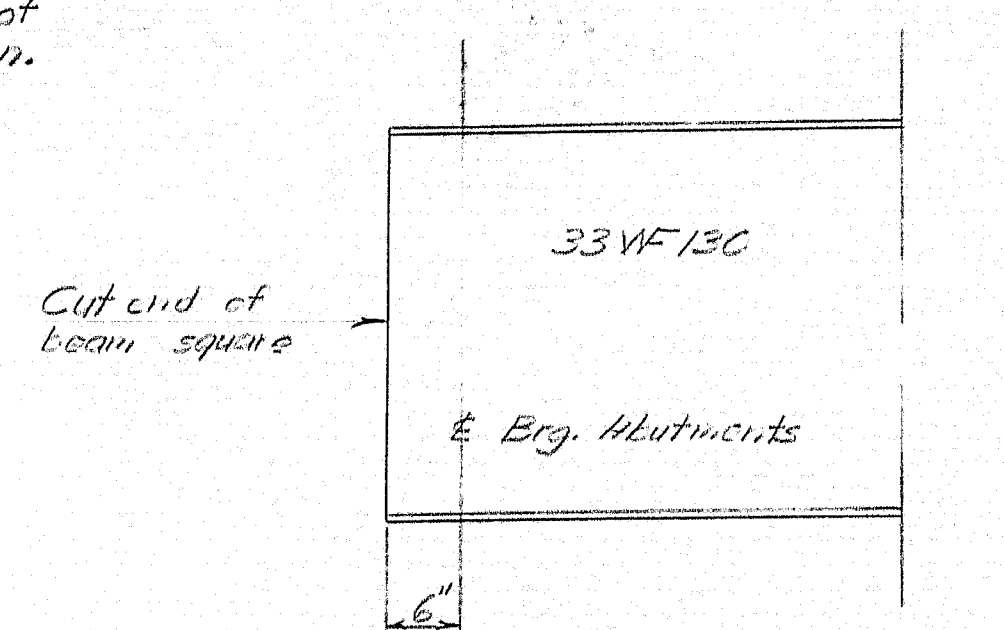
Diaphragms:
 D5 - 16 WF 36 10 Req'd.
 D6 - 15 C 33.9 11 Req'd.
 D7 - 16 WF 36 20 Req'd.
 D8 - 15 C 33.9 10 Req'd.

ERECTION DIAGRAM

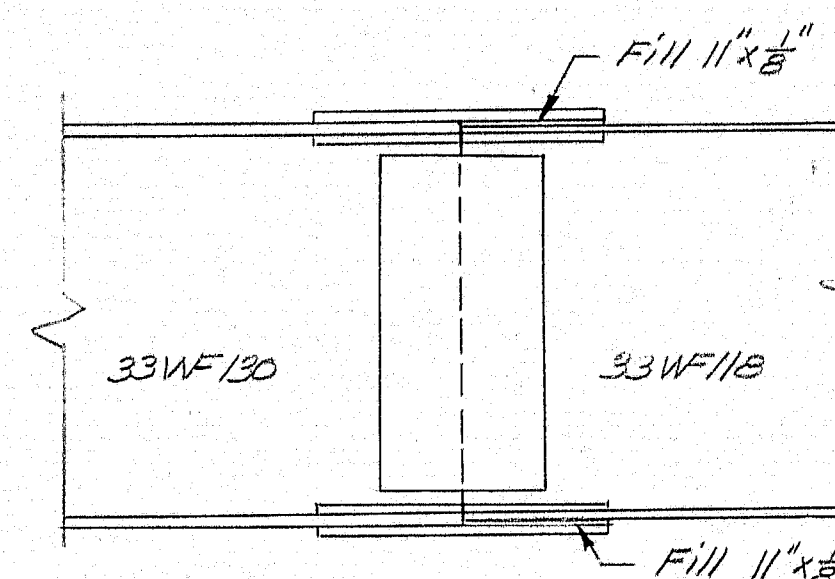


BRACKET PLATE DETAILS

Bracket plates in S3n shown.
 Bracket plates in S1n similar.



TYPICAL END OF STRINGER



TYPICAL SPLICE
 Standard Splice 33 WF 118

STRINGER GRADES IN PERCENT			
Line	S1	S2	S3
G	1.607	1.429	1.253
H	1.591	1.413	1.236
J	1.575	1.398	1.219
K	1.559	1.383	1.201
M	1.543	1.364	1.184
N	1.526	1.346	1.166

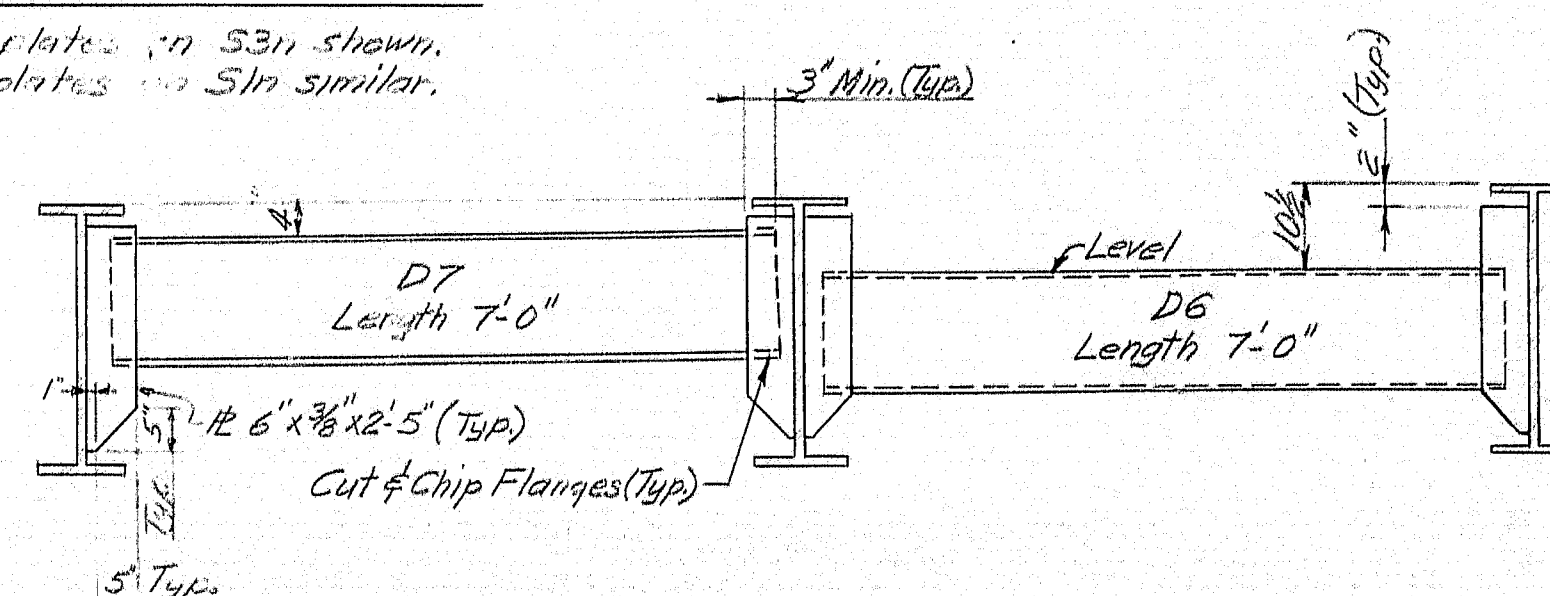
All grades are plus.

SPECIFICATIONS:
 Design - AASHTO Standard Specifications and revisions (1961).
 Fabrication & Erection - State of Maine Standard Specifications,
 Highway Bridges, Revision of Jan. 1956 with Supplemental
 Specifications.
 Materials - Steel stringers & splice plates shall be structural
 steel ASTM designation A36. All other structural steel
 shall be ASTM designation A7 or A36.

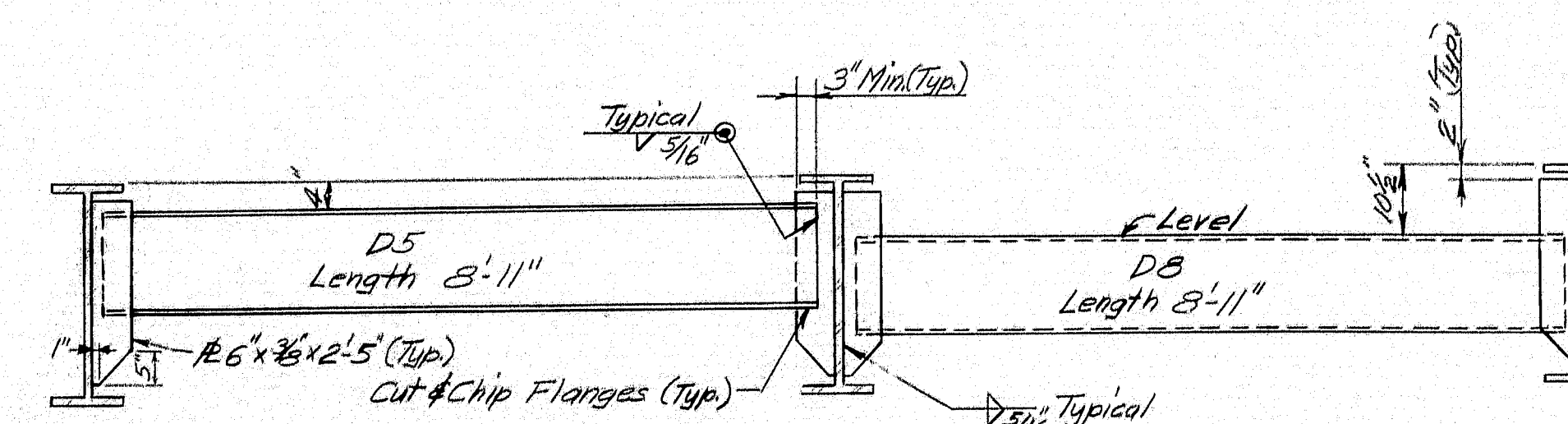
SPLICE NOTES:
 Fit the splice cut 33 WF 130 square and cut 33 WF 118 to match. Each
 row of bolts in the web splice shall be normal to the axis of 33 WF 130.
 Splice details are shown on Standard Details, E.D. 105-62, Beam Splices.
 Use standard splice for 33 WF 118.

BEARING NOTE:
 Bearing details are shown on Standard Details, E.D. 101-62, Bearing
 Pedestals. Use EPB-2 at Abutments 1 & 2 and Pier 2. Use
 EPB-2 at Pier 1.

GENERAL NOTES:
 Arched Joints and Drains are shown on Sheet 22.
 No camber all stringers and no cover plates required.
 All dimensions are horizontal.
 All diaphragms shall be set plumb.



DIAPHRAGM DETAILS
 Similar to details for D7-4.
 See Sheet 21



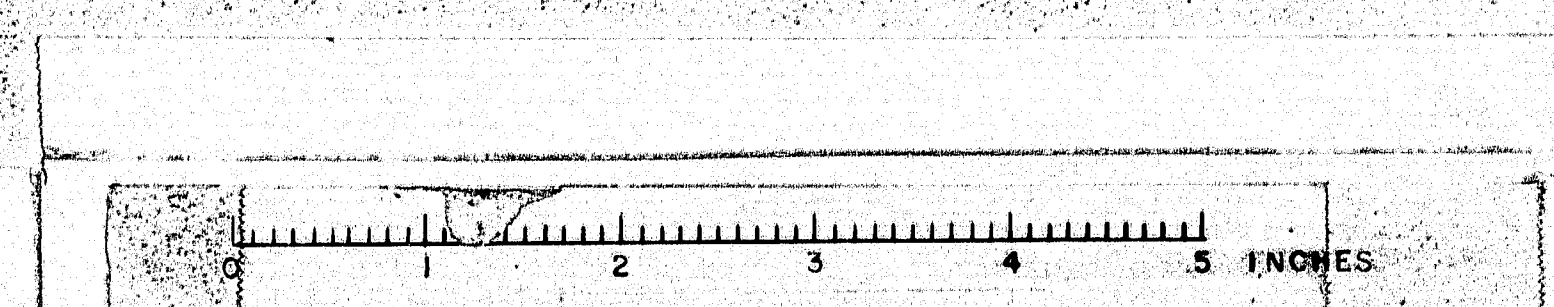
DESIGN-HRD
 TRACE-HRD
 CHECK-T/W.T.

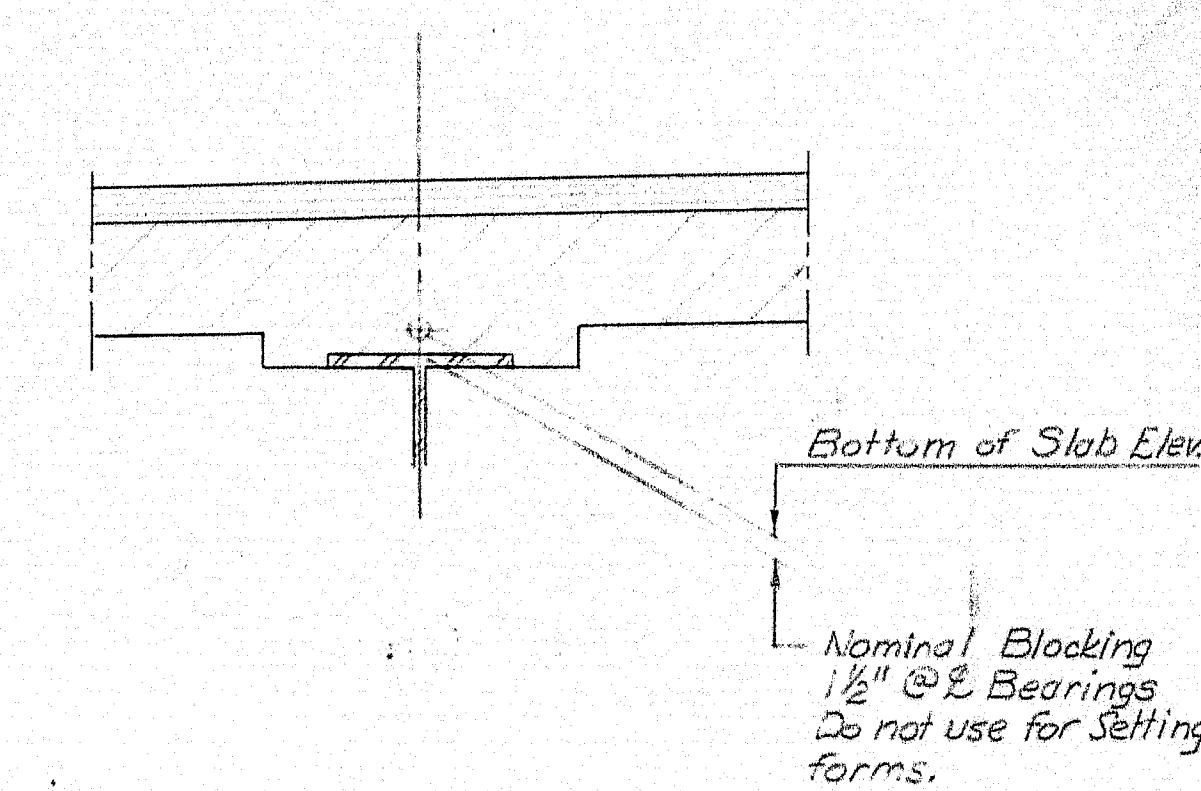
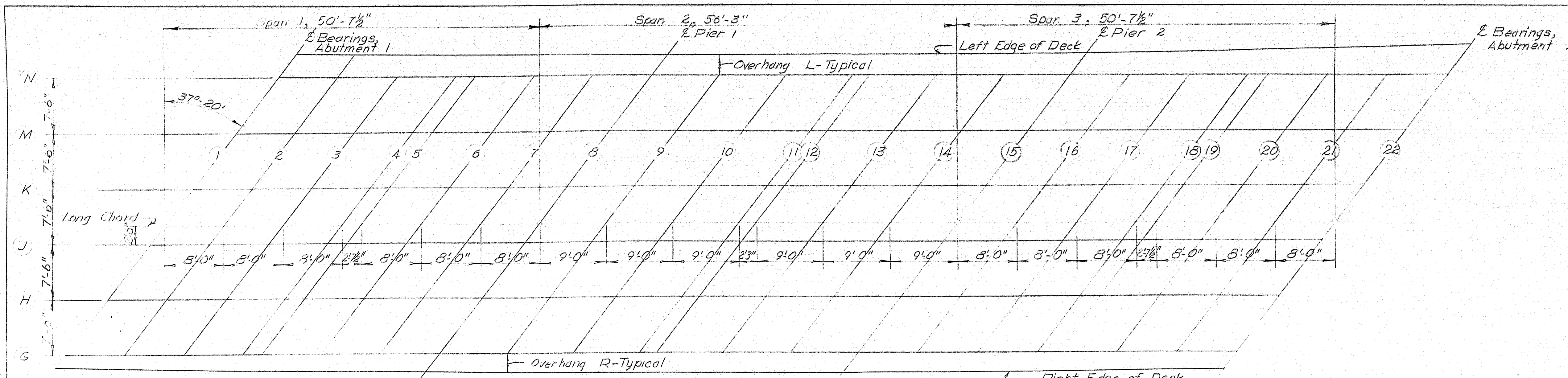
DET-HRD

STATE HIGHWAY COMMISSION
 BRIDGE DIVISION
 INTERSTATE 95 OVER RIVER ROAD
 IN THE TOWN OF
 BENTON
 KENNEBEC COUNTY
 STRUCTURAL STEEL S.B.

SHEET 20 OF 29 AUGUSTA MAINE JUNE 1963

(S.B.) Struct. Steel R. 90-20



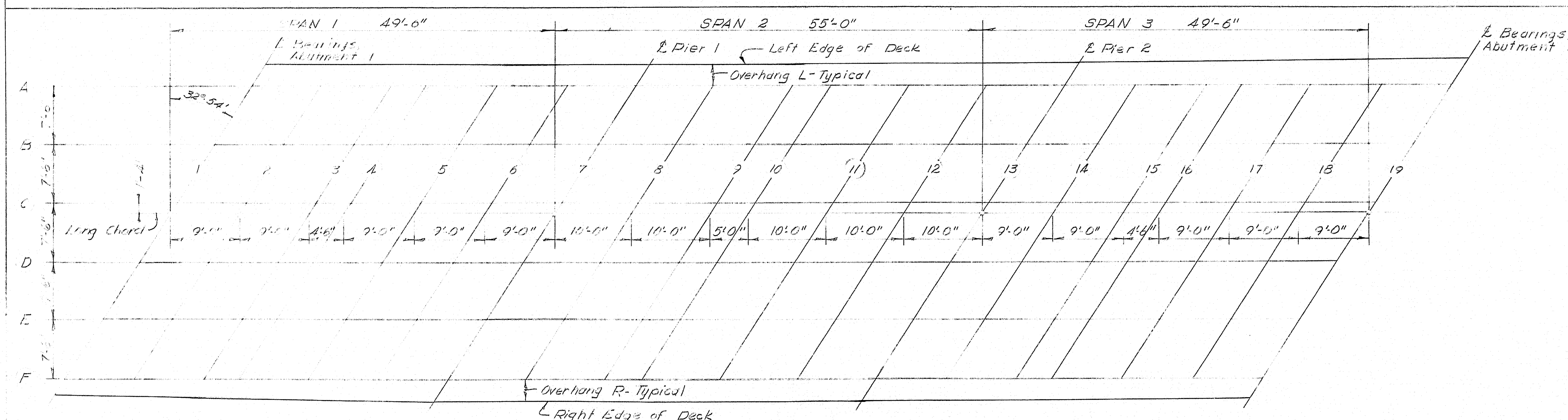


BLOCKING PLAN - SOUTHBOUND

BOTTOM OF SLAB ELEVATIONS & OVERHANG OF EDGE OF DECK																						
	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22
Overhang L	3'-1 ¹³ / ₈ "	3'-1 ¹ / ₂ "	3'-0 ³ / ₈ "	2'-11 ¹ / ₈ "	2'-11 ¹ / ₈ "	2'-10 ³ / ₈ "	2'-9 ³ / ₈ "	2'-9 ³ / ₈ "	2'-9 ³ / ₈ "	2'-9 ³ / ₈ "	2'-9 ³ / ₈ "	2'-9 ³ / ₈ "	2'-10 ³ / ₈ "	2'-10 ³ / ₈ "	2'-11 ¹ / ₈ "	3'-0 ³ / ₈ "	3'-2 ¹ / ₈ "	3'-3 ³ / ₈ "	3'-4"	3'-5 ³ / ₈ "	3'-7 ³ / ₈ "	3'-9 ³ / ₈ "
Line 1	142.15	142.23	142.37	142.49	142.53	142.64	142.75	142.86	142.99	143.12	143.25	143.28	143.40	143.50	143.62	143.72	143.83	143.94	143.97	144.06	144.13	144.21
" M	142.29	142.35	142.49	142.61	142.65	142.77	142.87	142.98	143.12	143.25	143.38	143.41	143.53	143.64	143.75	143.86	143.97	144.07	144.11	144.20	144.28	144.35
" K	142.32	142.47	142.61	142.73	142.77	142.89	143.00	143.11	143.24	143.38	143.51	143.54	143.66	143.77	143.89	144.00	144.11	144.21	144.25	144.34	144.42	144.49
" J	142.44	142.58	142.72	142.85	142.89	143.01	143.12	143.23	143.37	143.51	143.64	143.67	143.79	143.90	144.02	144.13	144.24	144.35	144.38	144.48	144.56	144.64
" H	142.55	142.70	142.84	142.97	143.01	143.13	143.24	143.36	143.49	143.63	143.77	143.80	143.92	144.03	144.15	144.26	144.38	144.49	144.52	144.62	144.70	144.78
" G	142.67	142.82	142.96	143.09	143.13	143.25	143.36	143.48	143.62	143.76	143.89	143.93	144.05	144.17	144.29	144.40	144.52	144.62	144.66	144.76	144.84	144.92
Overhang R	1'-9 ³ / ₈ "	1'-11 ¹ / ₈ "	2'-0 ³ / ₈ "	2'-2 ³ / ₈ "	2'-2 ³ / ₈ "	2'-4 ¹ / ₈ "	2'-5 ³ / ₈ "	2'-6 ³ / ₈ "	2'-7 ³ / ₈ "	2'-7 ³ / ₈ "	2'-8 ³ / ₈ "	2'-8 ³ / ₈ "	2'-8 ³ / ₈ "	2'-8 ³ / ₈ "	2'-8 ⁷ / ₈ "	2'-8"	2'-7 ³ / ₈ "	2'-6 ³ / ₈ "	2'-6 ³ / ₈ "	2'-5 ³ / ₈ "	2'-4 ¹ / ₈ "	2'-2 ¹⁵ / ₁₆ "

Note: In order that the roadway slab will conform to the profile and cross sections shown on these plans the accompanying table of Elevations is given. Elevations for the bottom of the slab, which are computed to compensate for dead load deflections, must be set before slab forms are started.

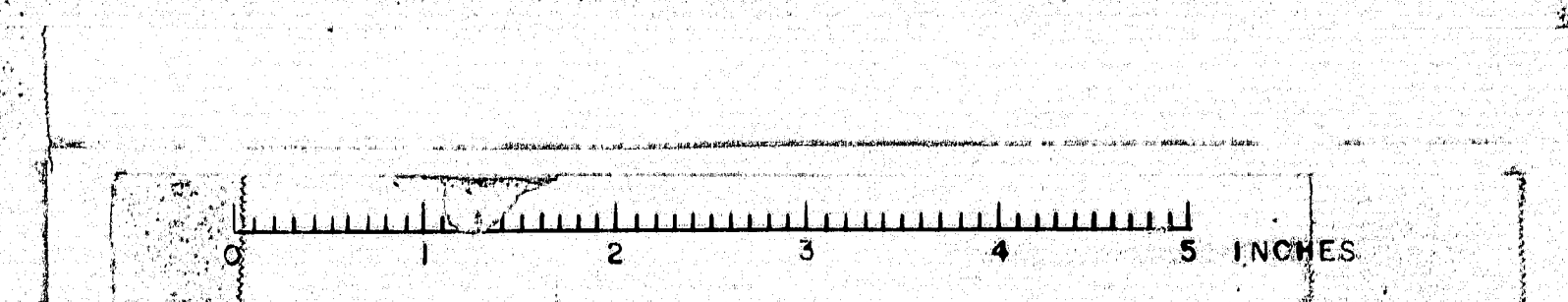
BLOCKING DETAIL
Northbound and Southbound

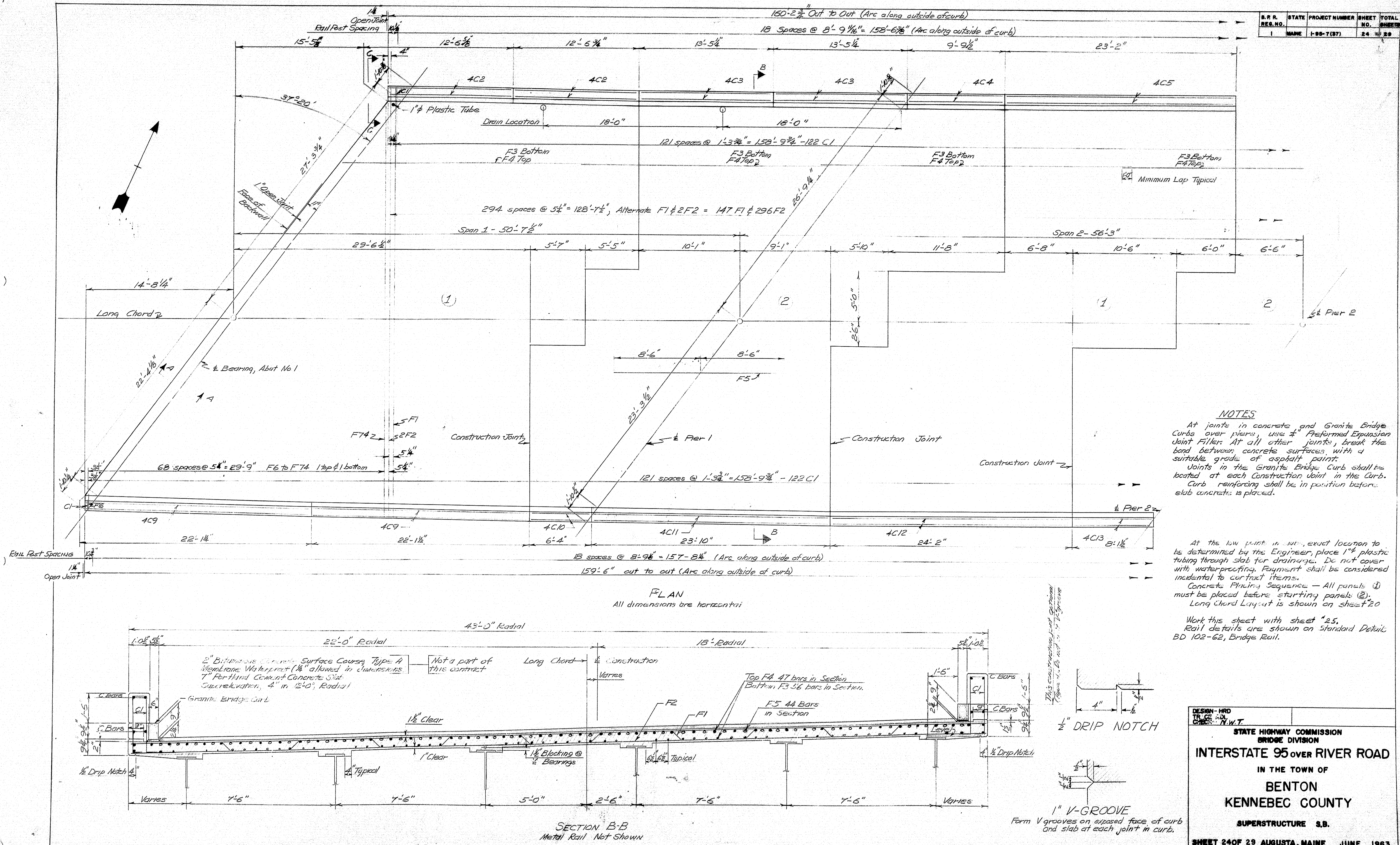


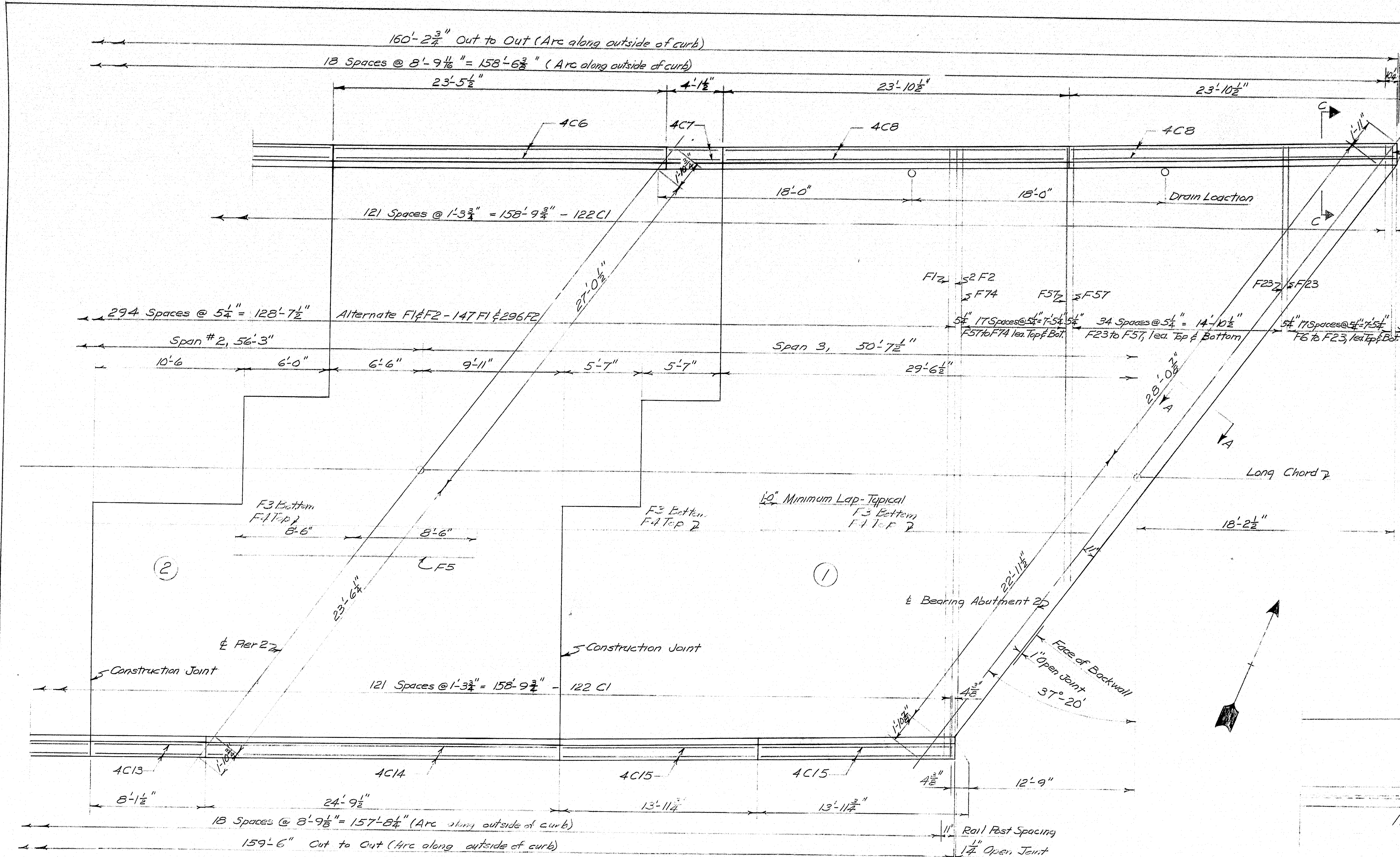
BLOCKING PLAN - NORTHBOUND

BOTTOM of SLAB ELEVATIONS & OVERHANG of EDGE of DECK																			
	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19
Overhang L	2'-11 ¹³ / ₁₆ "	2'-10 ⁵ / ₈ "	2'-9 ¹ / ₂ "	2'-8 ⁹ / ₁₆ "	2'-7 ⁵ / ₈ "	2'-6 ³ / ₈ "	2'-6 ³ / ₈ "	2'-6 ¹ / ₈ "	2'-6"	2'-6 ¹ / ₈ "	2'-6 ⁷ / ₁₆ "	2'-7 ¹ / ₈ "	2'-7 ³ / ₈ "	2'-8 ⁵ / ₈ "	2'-10 ¹ / ₈ "	2'-10 ¹³ / ₁₆ "	3'-0 ³ / ₈ "	3'-2 ⁵ / ₈ "	3'-4 ¹ / ₈ "
Line A	149.12	149.25	149.44	149.51	149.65	149.78	149.91	150.06	150.21	150.29	150.43	150.56	150.70	150.83	150.96	151.02	151.14	151.24	151.34
" B	149.25	149.42	149.57	149.65	149.78	149.91	150.04	150.20	150.35	150.43	150.57	150.70	150.84	150.97	151.10	151.17	151.28	151.39	151.48
" C	149.33	149.55	149.70	149.78	149.91	150.04	150.18	150.33	150.49	150.56	150.71	150.84	150.98	151.11	151.24	151.31	151.43	151.53	151.63
" D	149.51	149.67	149.83	149.91	150.05	150.18	150.31	150.47	150.62	150.70	150.85	150.98	151.12	151.25	151.39	151.45	151.57	151.68	151.78
" E	149.63	149.80	149.96	150.04	150.18	150.31	150.44	150.60	150.76	150.84	150.98	151.12	151.26	151.39	151.53	151.60	151.72	151.82	151.92
" F	149.76	149.93	150.09	150.17	150.31	150.44	150.58	150.74	150.90	150.97	151.12	151.26	151.40	151.54	151.67	151.74	151.86	151.97	152.07
Overhang R	2'-1 ¹ / ₄ "	2'-3 ³ / ₈ "	2'-5 ³ / ₈ "	2'-6"	2'-7 ⁷ / ₁₆ "	2'-8 ³ / ₄ "	2'-9 ¹³ / ₁₆ "	2'-10 ¹ / ₈ "	2'-11 ³ / ₁₆ "	2'-11 ⁵ / ₈ "	2'-11 ⁵ / ₈ "	3'-0"	2'-11 ³ / ₄ "	2'-11 ³ / ₈ "	2'-10 ¹ / ₈ "	2'-10 ⁵ / ₁₆ "	2'-9 ⁵ / ₁₆ "	2'-8 ³ / ₄ "	2'-6 ¹ / ₂ "

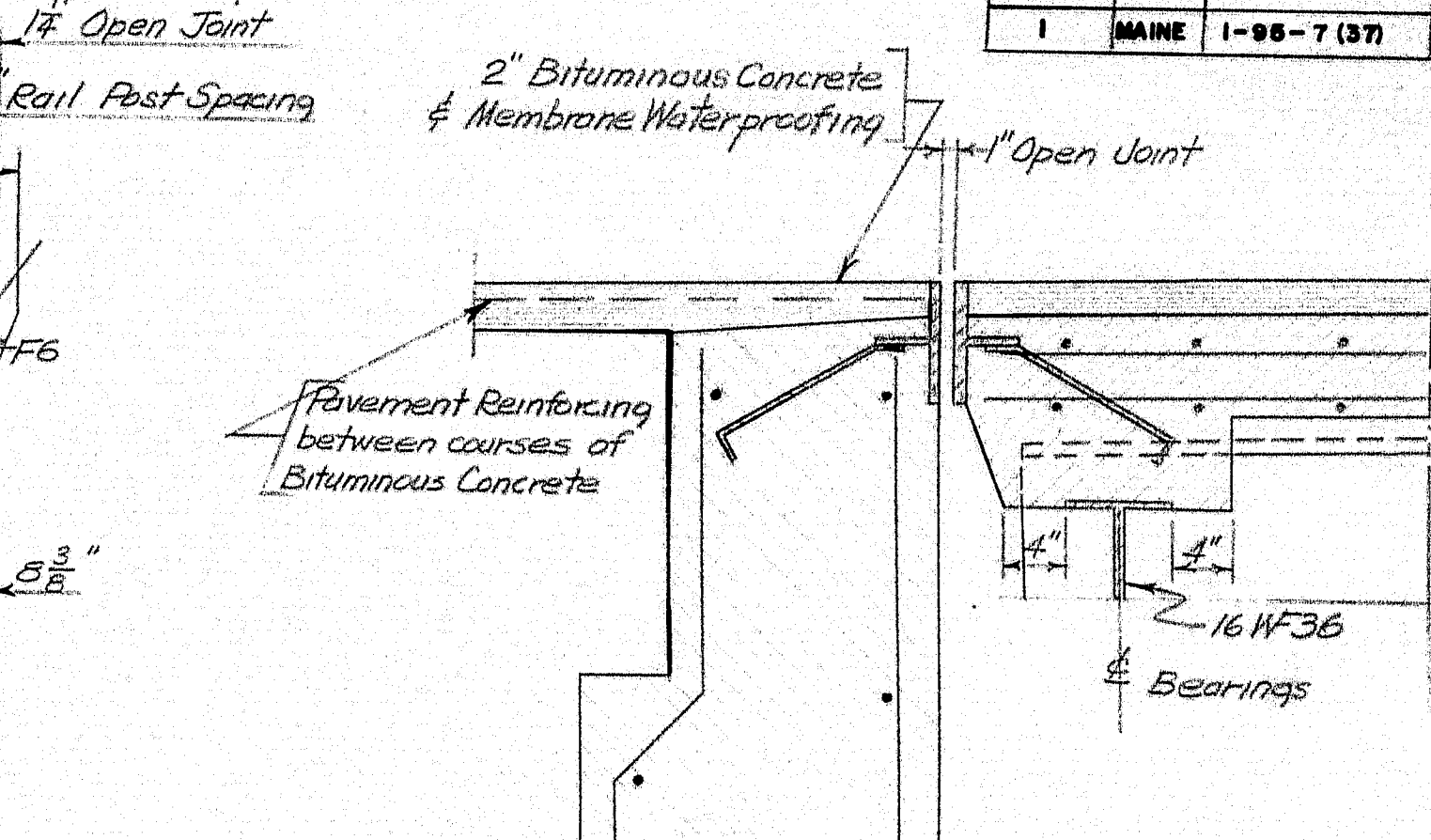
DES-AMR & MRO
TRACE-EB
CHECK-N.W.T.
DET-EB
STATE HIGHWAY COMMISSION
BRIDGE DIVISION
INTERSTATE 95 OVER RIVER ROAD
IN THE TOWN OF
BENTON
KENNEBEC COUNTY
BLOCKING SB & NB
SHEET 23 OF 29 AUGUSTA MAINE JUNE 1963





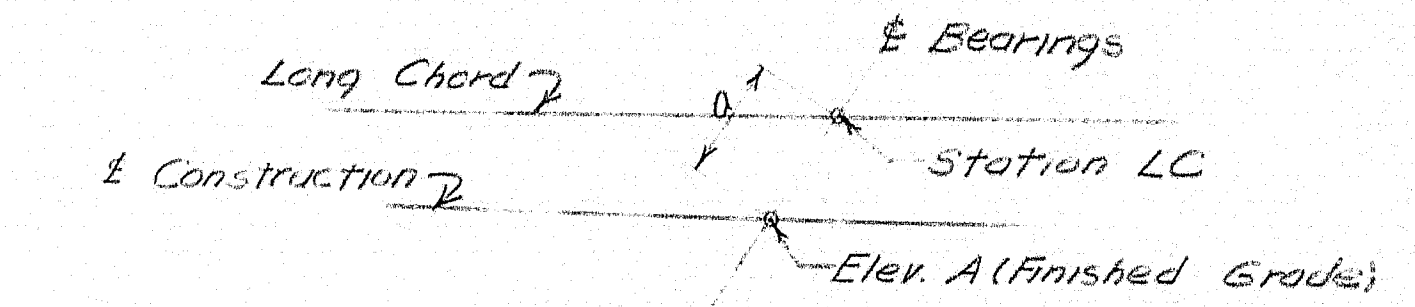


PLAN
All dimensions are horizontal

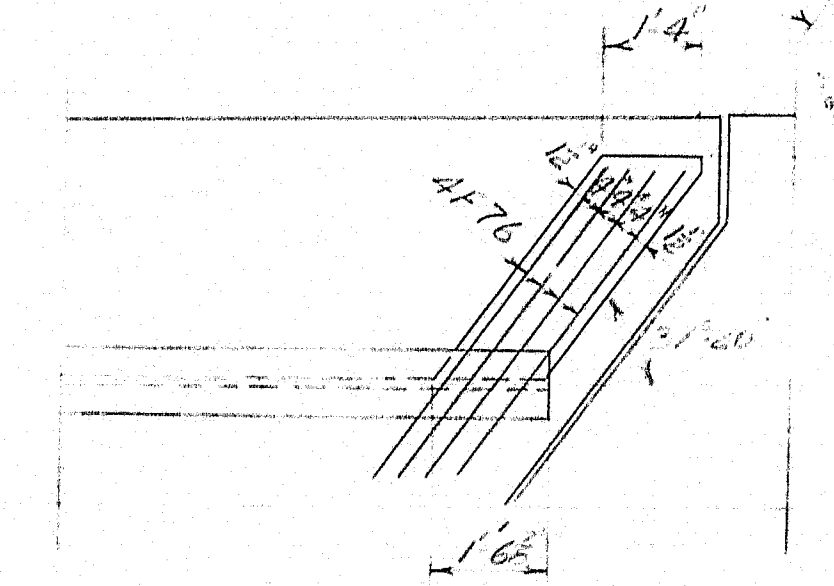


SECTION A-A

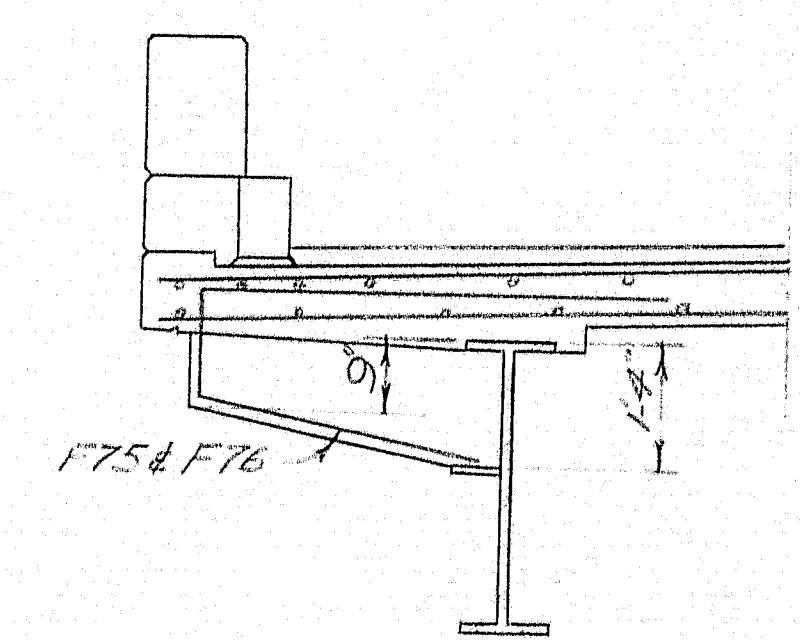
NOTE: Pavement Reinforcing Details - 36" minimum width x 30'-0"±, 4"x4"-10 gage galvanized welded wire mesh. (An equivalent, satisfactory to the Engineer, may be substituted if 44-1010 is not readily available.) Bituminous Concrete, Membrane Waterproofing, and Pavement Reinforcing are not a part of this Contract.



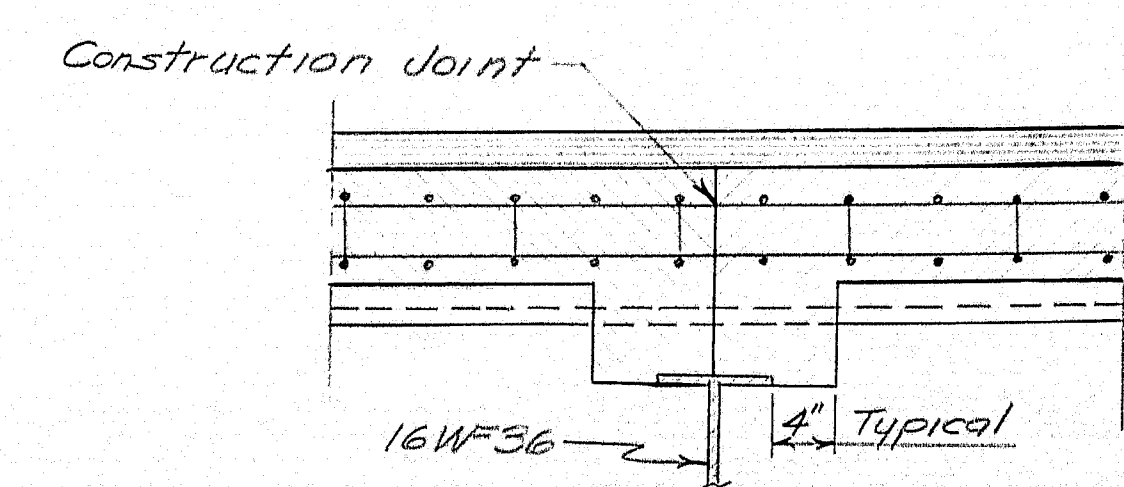
± BEARINGS	STATION LC	A	a
Abutment 1	1287 + 01.03	143.17	9 3/8"
± Pier 1	1287 + 51.66	143.93	9 3/8"
± Pier 2	1288 + 07.91	144.76	9 3/8"
Abutment 2	1288 + 58.53	145.36	9 3/8"



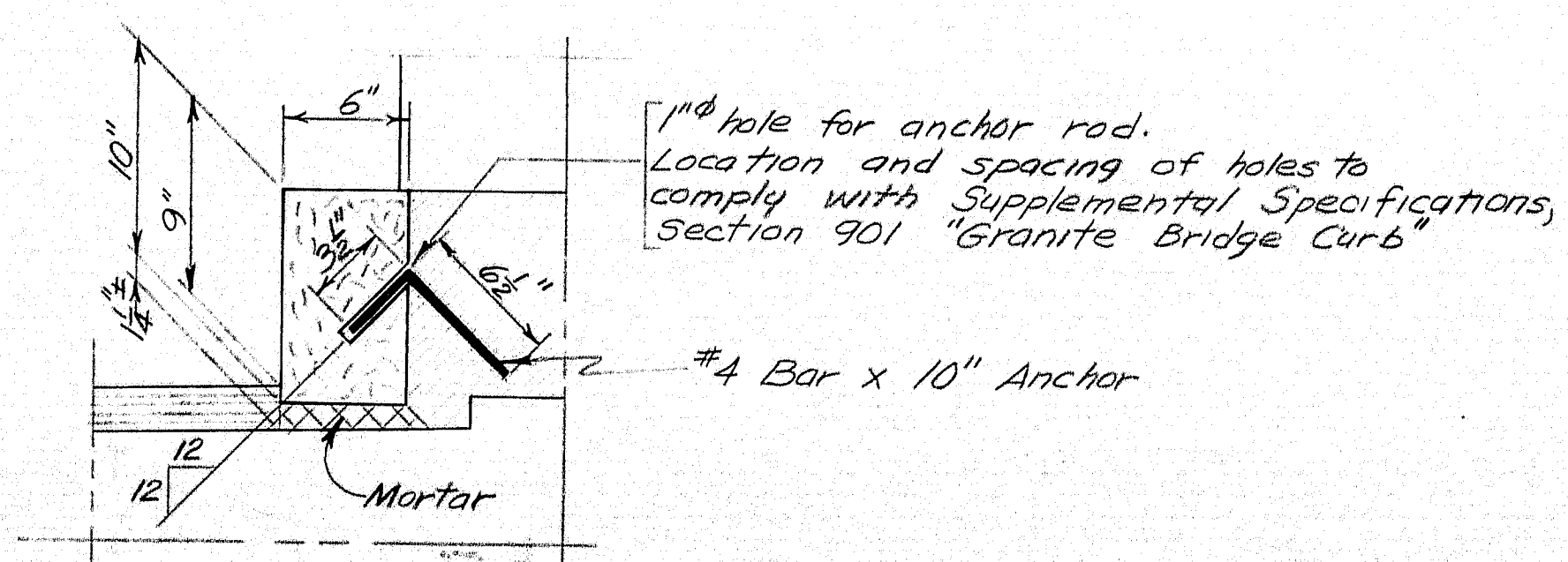
BRACKET PLAN, SPAN 3



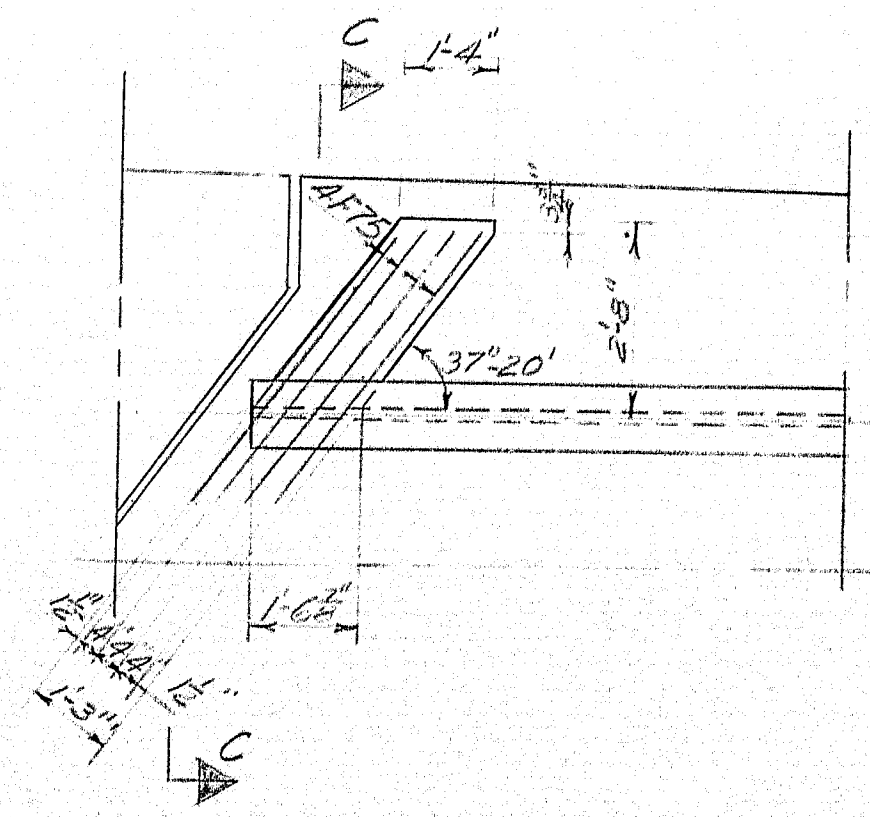
SECTION CC



TYPICAL SLAB
CONSTRUCTION JOINT



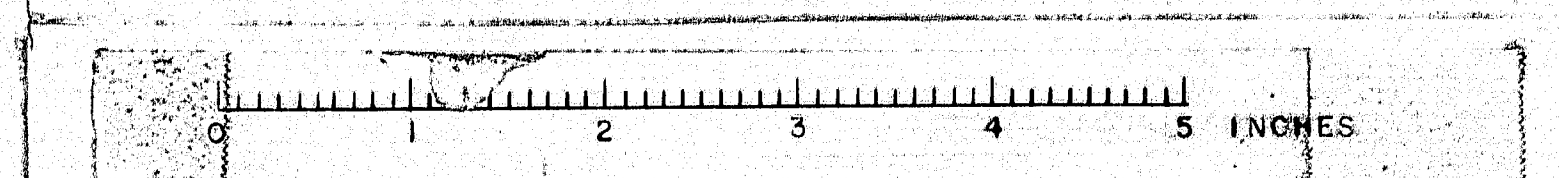
TYPICAL GRANITE
BRIDGE CURB

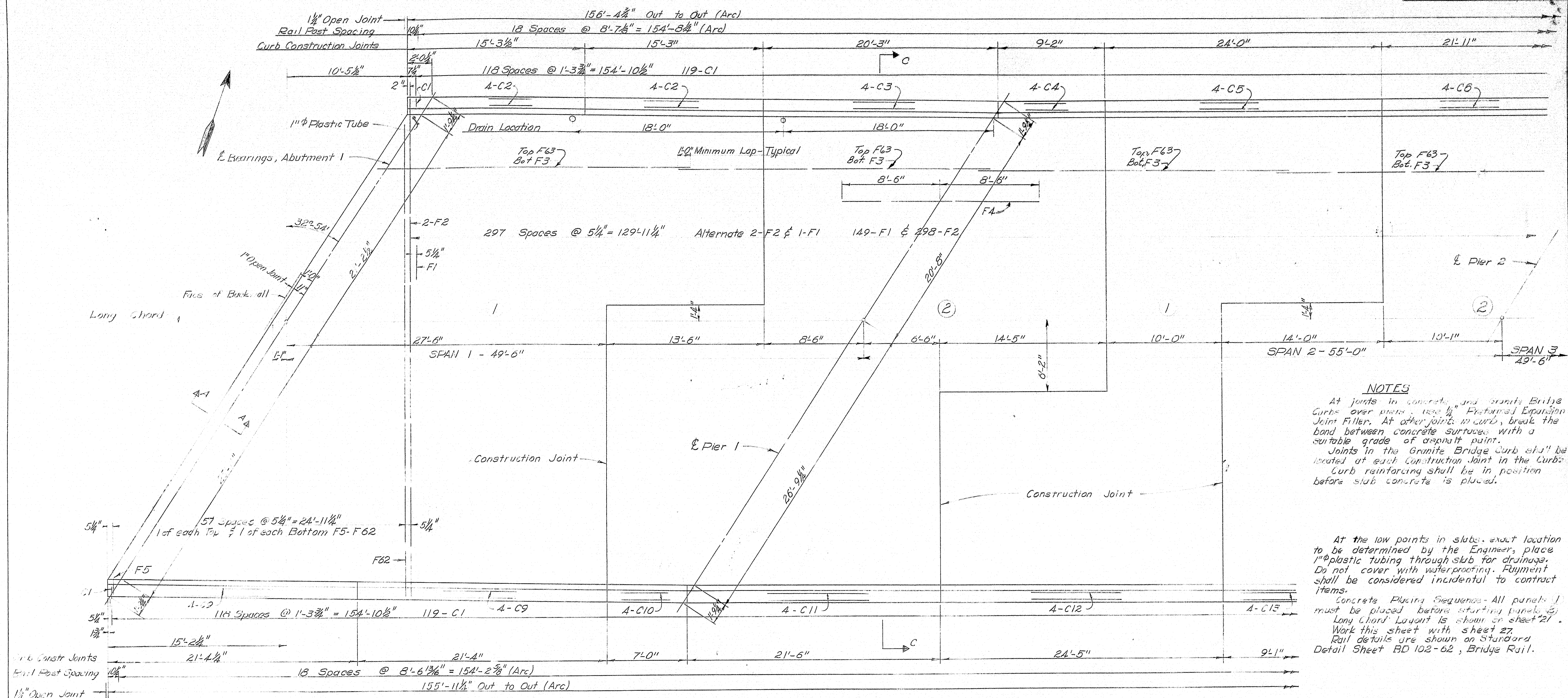


BRACKET PLAN, SPAN 1

DESIGN: HPD
TRACE: JDL
CHECK: N.W.T.

STATE HIGHWAY COMMISSION
BRIDGE DIVISION
INTERSTATE 95 OVER RIVER ROAD
IN THE TOWN OF
BENTON
KENNEBEC COUNTY
SUPERSTRUCTURE S.B.
SHEET 25 OF 29 AUGUSTA, MAINE, JUNE 1963
Supers. S.B.





NOTES

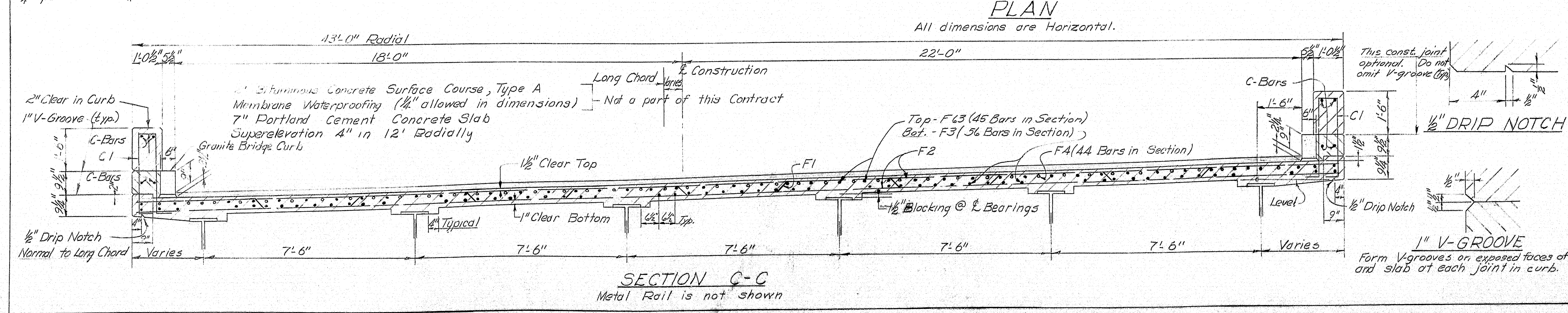
At joints in concrete and granite Bridge Curb over piers, use 1/2" Extruded Expansion Joint Filler. At other joints in curb, break the band between concrete surfaces with a suitable grade of asphalt paint.

Joints in the Granite Bridge Curb shall be located at each Construction Joint in the Curb. Curb reinforcing shall be in position before slab concrete is placed.

At the low points in slabs, exact location to be determined by the Engineer, place 1" plastic tubing through slab for drainage. Do not cover with waterproofing. Payment shall be considered incidental to contract items.

Concrete Placing Sequence - All panels must be placed before starting panels 25. Long Chord Layout is shown on sheet 27. Work this sheet with sheet 27.

Rail details are shown on Standard Detail Sheet BD 102-62, Bridge Rail.

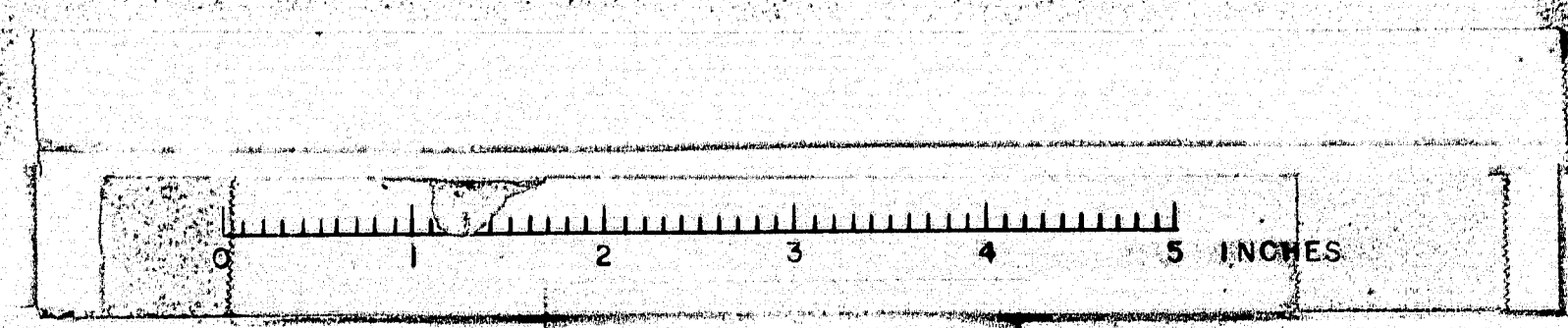


DESIGN-AHR
TRACE-EB
CHECK-N.W.T.

STATE HIGHWAY COMMISSION
BRIDGE DIVISION
INTERSTATE 95 OVER RIVER ROAD
IN THE TOWN OF
BENTON
KENNEBEC COUNTY
SUPERSTRUCTURE N.B.

SHEET 26 OF 29 AUGUSTA MAINE JUNE 1963

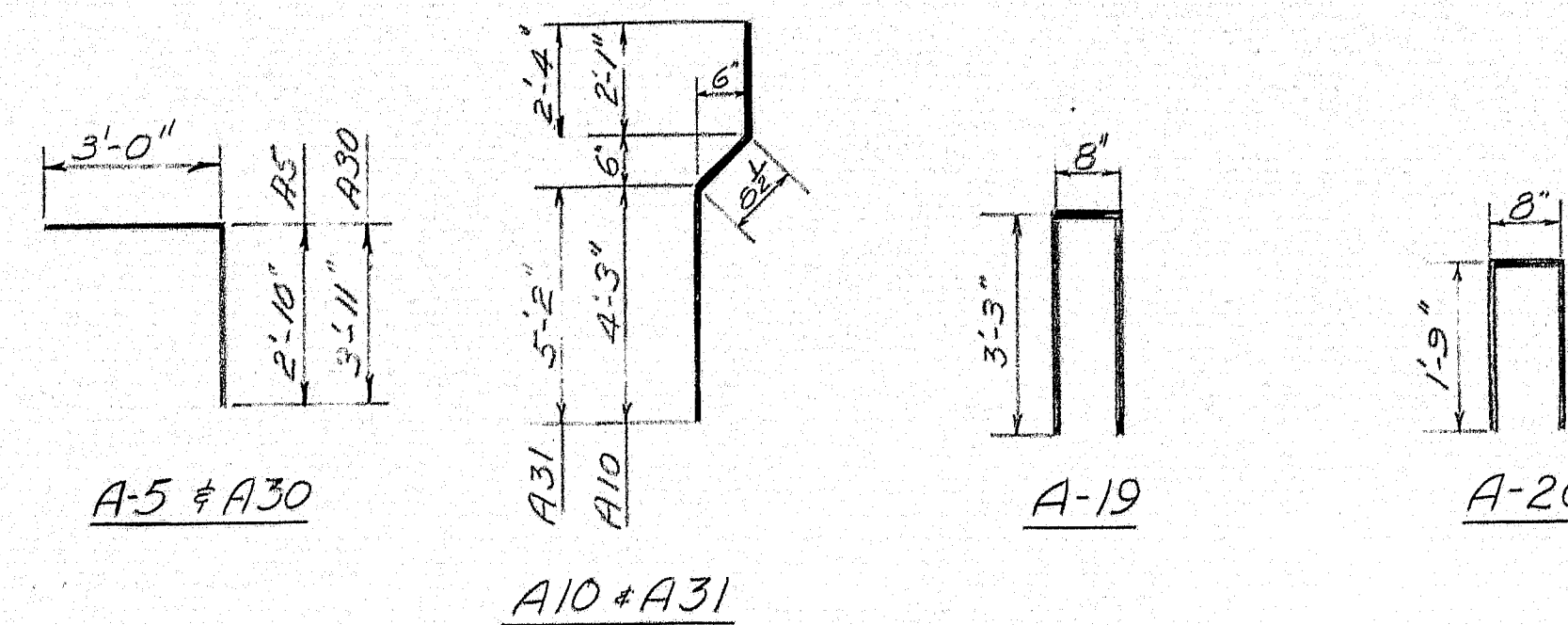
Supra M.B. River Rd. - Benton 90-26



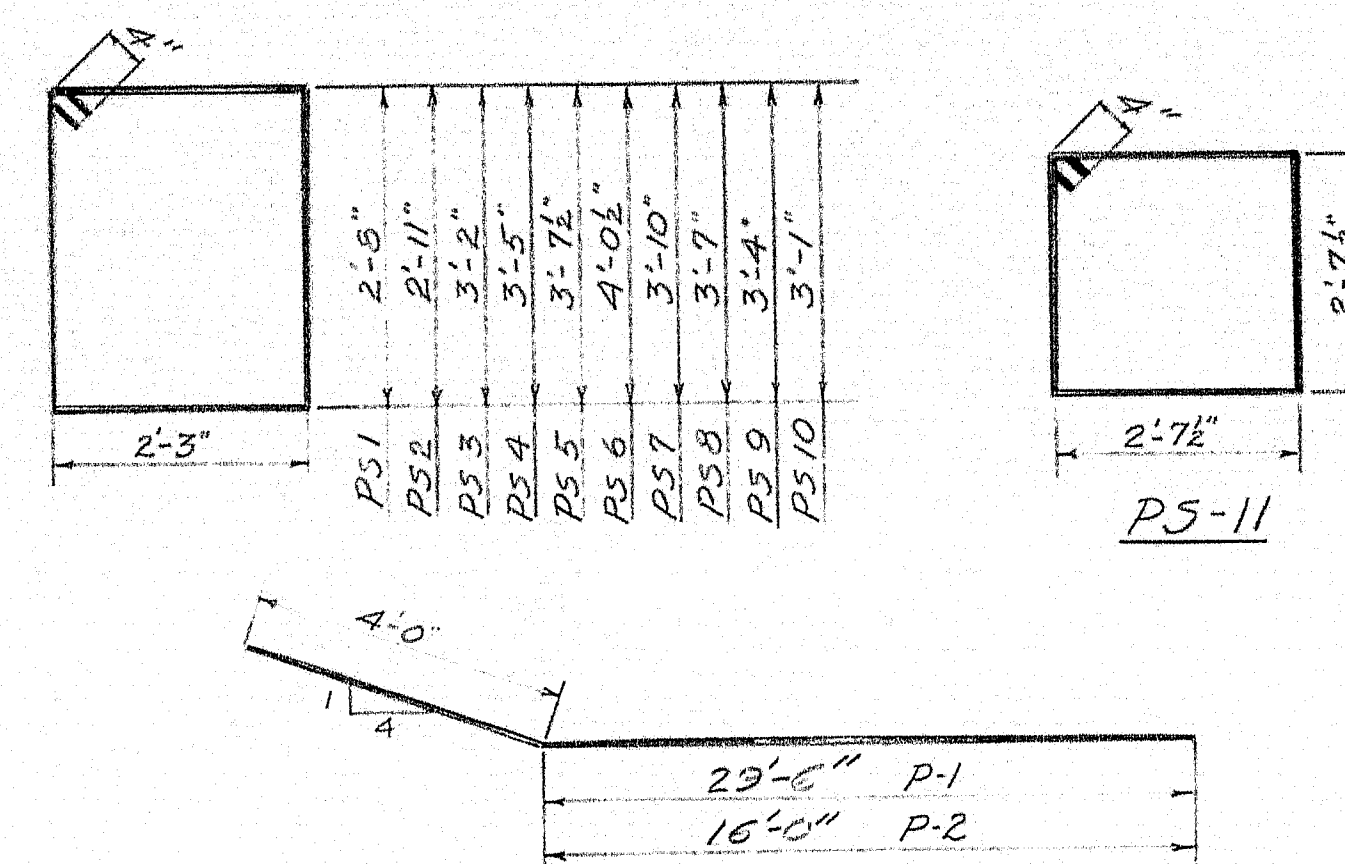
REINFORCING STEEL SCHEDULE - SOUTH BOUND

STATE PROJECT NUMBER SHEET TOTAL
1 MAINE 1-95-7(37) 28 29

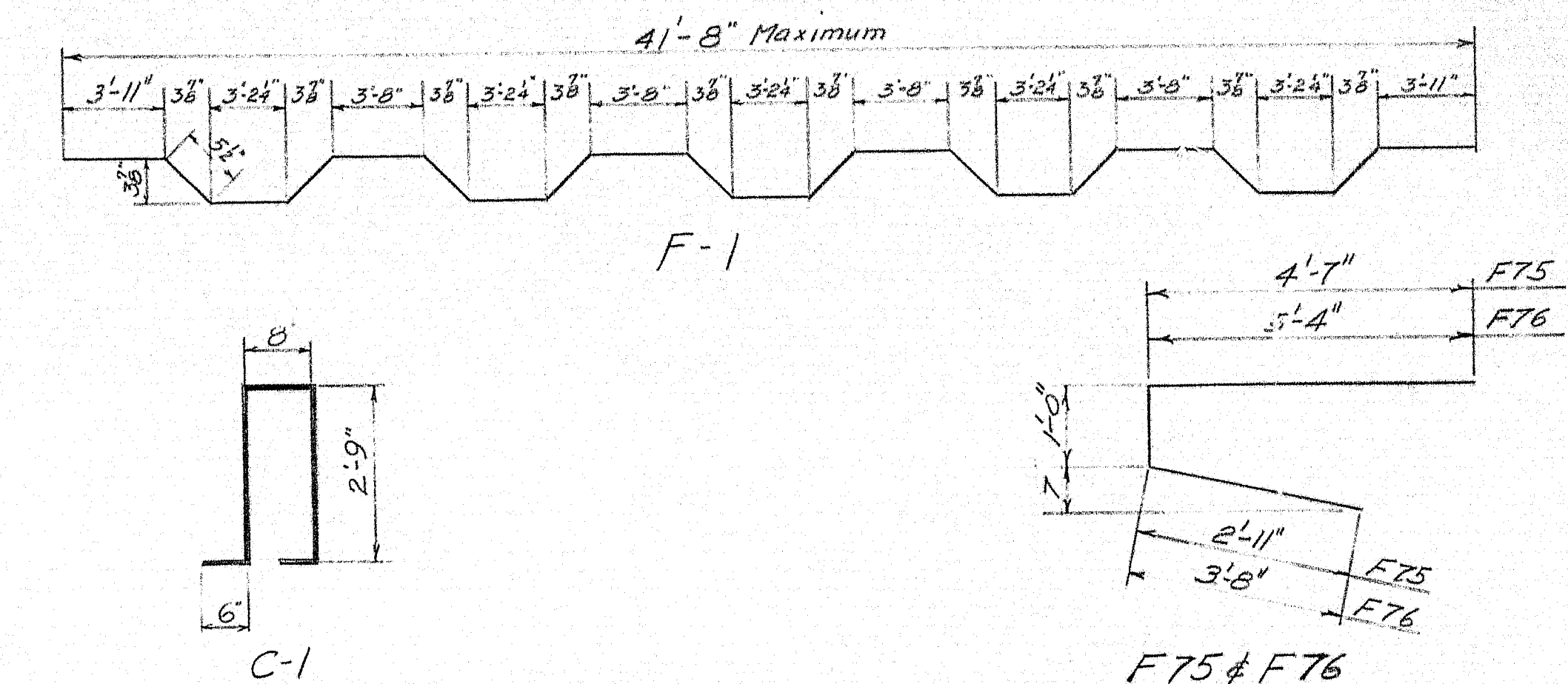
ABUTMENTS



PIERS



SUPERSTRUCTURE



ABUTMENT #1					ABUTMENT #2					PIER #1					PIER #2					SUPERSTRUCTURE				
BENT BARS					BENT BARS					BENT BARS					BENT BARS					BENT BARS				
MARK	SIZE	NUMBER	LENGTH	LOCATION	MARK	SIZE	NUMBER	LENGTH	LOCATION	MARK	SIZE	NUMBER	LENGTH	LOCATION	MARK	SIZE	NUMBER	LENGTH	LOCATION	MARK	SIZE	NUMBER	LENGTH	LOCATION
A5	#5	34	5'-10"	Breastwall	A19	#4	20	7'-2"	Wing Walls	PS1	#4	2	10'-6"	Pier Cap	PS1	#4	2	10'-6"	Pier Cap	F1	#5	147	43'-0"	Slab, Trans.
A10	#5	33	7'-0 1/2"	Backwall	A20	#4	6	4'-2"	End Posts	PS2	#4	2	11'-6"		PS2	#4	2	11'-6"		F75	#5	4	8'-6"	Slab Bracket, Span 1
A19	#4	20	7'-2"	Wing Walls	A30	#5	34	5'-11"	Breastwall	PS3	#4	2	11'-6"		PS3	#4	2	11'-6"		F76	#5	4	10'-0"	Slab Bracket, Span 3
A20	#4	6	4'-2"	End Posts	A31	#5	34	8'-2 1/2"	Backwall	PS4	#4	2	12'-0"		PS4	#4	2	12'-0"		STRAIGHT BARS				
STRAIGHT BARS					STRAIGHT BARS					STRAIGHT BARS					STRAIGHT BARS					STRAIGHT BARS				
A1	#6	102	5'-1"	Footings	A1	#6	104	5'-1"	Footings	PS9	#4	2	11'-10"		PS9	#4	2	11'-10"		F2	#5	296	42'-8"	Slab Trans.
A2	#6	12	24'-0"	"	A4	#5	68	3'-0"	"	PS10	#4	2	11'-4"		PS10	#4	2	11'-4"		F3	#5	336	27'-5"	Slab Trans.
A3	#6	12	29'-0"	"	A6	#5	3	25'-6"	Bridge Seat	PS11	#4	2	11'-2"	Columns	PS11	#4	2	11'-2"	Columns	F4	#4	282	27'-5"	"
A4	#5	67	3'-0"	"	A7	#5	3	28'-0"	"	P1	#8	7	33'-6"	Btm. Pier Cap	P1	#8	7	33'-6"	Btm. Pier Cap	F5	#5	88	17'-0"	" over piers
A6	#5	17	25'-6"	Breastwall & Backwall	A11	#5	33	4'-9"	Backwall	P2	#8	7	20'-0"	"	P2	#8	7	20'-0"	"	F6	#4	4	1'-5"	Trans. Ends
A7	#5	17	28'-0"	"	A12	#6	34	3'-6"	Slab Seat											F7	#5	4	2'-2"	"
					A13	#6	52	2'-6"	Fig., Both Wings											F8	#5	4	2'-9"	"
					A14	#6	8	11'-8"	" , Northerly Wing											F9	#5	4	3'-3"	"
					A15	#5	42	2'-4"	" , Both Wings											F10	#5	4	3'-10"	"
					A17	#5	10	11'-1"	Northerly Wing	STRAIGHT BARS					STRAIGHT BARS					F11	#5	4	4'-5"	"
					A18	#5	20	9'-8"	Both Wings	P3	#6	8	26'-6"	Sides Pier Cap	P3	#6	8	26'-6"	Sides Pier Cap	F12	#5	4	5'-0"	"
					A21	#4	4	9'-8"	"	P5	#8	8	26'-6"	Top Pier Cap	P5	#8	8	26'-6"	Top Pier Cap	F13	#5	4	5'-7"	"
					A22	#4	4	2'-2"	"	P6	#9	6	26'-6"	"	P6	#9	6	26'-6"	"	F14	#5	4	6'-2"	"
					A23	#5	20	7'-4"	Southerly Wing											F15	#5	4	6'-9"	"
					A24	#5	2	2'-10"	Northerly Wing	P11	#6	48	5'-6"	Footings	P11	#6	48	5'-6"	Footings	F16	#5	4	7'-4"	"
					A25	#5	8	12'-3"	"	Dx2	#6	64	9'-8 1/2"	Dowels, Fig.	Dx1	#8	64	21'-0"	Columns	F17	#5	4	7'-11"	"
					A27	#6	8	13'-2"	Footings, Southerly Wing	P22	#8	16	15'-6"	Columns	Dx1	#8	64	21'-0"	Columns	F18	#5	4	8'-5"	"
					A28	#5	20	8'-10"	"	P23	#8	16	16'-6"	"						F19	#5	4	9'-0"	"
					A32	#5	14	25'-0"	Breastwall & Backwall	P24	#8	16	18'-6"	"						F20	#5	4	9'-7"	"
					A33	#5	14	29'-0"	"	P25	#8	16	17'-6"	"						F21	#5	4	10'-1"	"
					A34	#6	12	25'-3"	Footings											F22	#5	4	10'-8"	"
					A35	#6	12	29'-0"	"											F23	#5	4	11'-3"	"
																				F24	#5	4	12'-1"	"
																				F25	#5	4	12'-5"	"
																				F26	#5	4	13'-3"	"
																				F27	#5	4	13'-10"	"
																				F28	#5	4	14'-5"	"
																				F29	#5	4	15'-0"	"
																				F30	#5	4	15'-7"	"
																				F31	#5	4	16'-2"	"
																				F32	#5	4	16'-9"	"
																				F33	#5	4	17'-4"	"
																				F34	#5	4	17'-11"	"
																				F35	#5	4	18'-6"	"
																				F36	#5	4	19'-1"	"
																				V.T.H.K.				
															</									

V.A.B.P.

NOTES

DIMENSIONS ARE TO C OF BARS.
ALL REINFORCING STEEL SHALL BE
INTERMEDIATE GRADE.

Note: As built revisions shown in
black ink. 9-24-64 E. Barnard

DESIGN - H.R.G.
TRACE - F.B.P.
CHECK - R.S. NOTED

BRIDGE NO.
SURVEY -
PLOT -

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

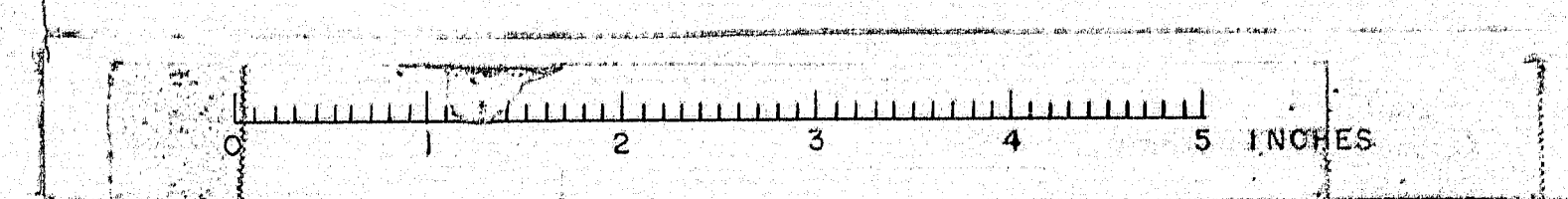
INTERSTATE 95 OVER RIVER ROAD

IN THE TOWN OF
BENTON
KENNEBEC COUNTY

REINFORCING STEEL SCHEDULE SB

SHEET 28 OF 29 AUGUSTA, MAINE JUNE 1963

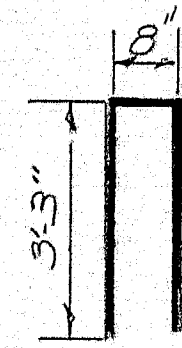
Rural Steel Schedules - River Rd 90-28



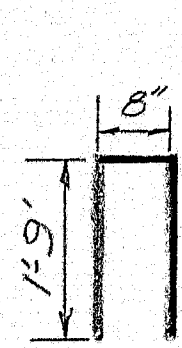
B. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-7(37)	22	22

PIER 3

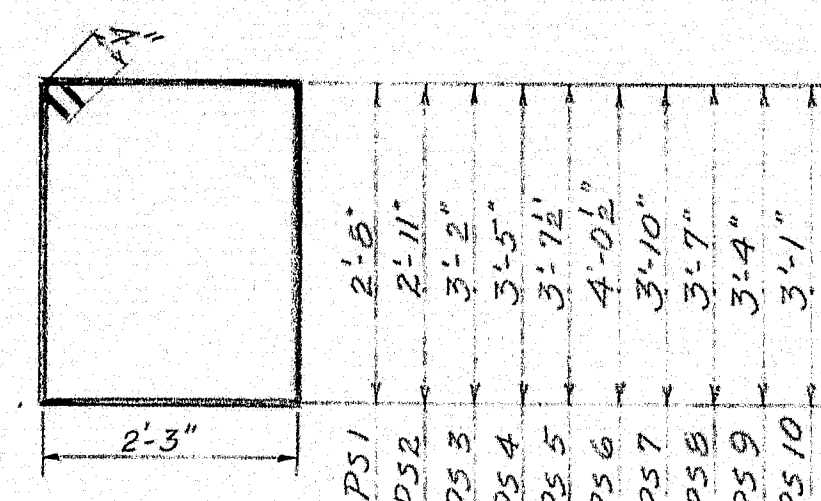
Hand-drawn diagram of a roof truss section. The diagram shows a horizontal line labeled "Hor. Leg" with a dimension of 3'-0". Below this line, there are two vertical dimensions: 2'-10" and 6'-0". To the right of these dimensions, there are two labels: A11 and A35. Further to the right, there is a vertical dimension of 7'-1" and a horizontal dimension of 4'-3". Below these dimensions, there are two labels: A34 and A12. At the top right, there is a dimension of 6'-2-5/8" and a label B1. Below this, there is a dimension of 6'-1-1/2" and a label B2. At the bottom right, there is a dimension of 6'-0" and a label B3. The diagram is drawn on a grid.



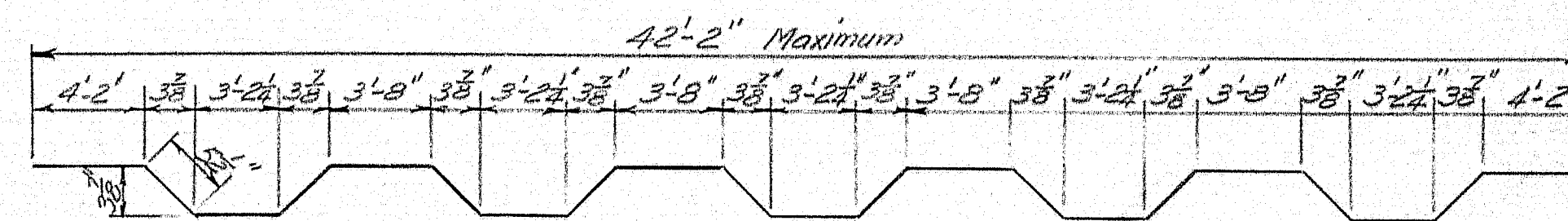
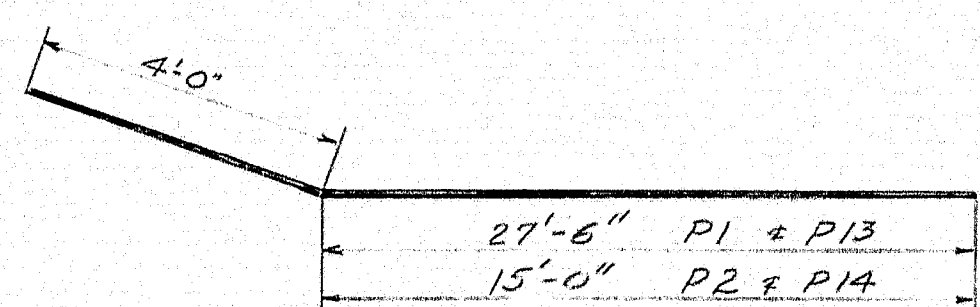
A-19



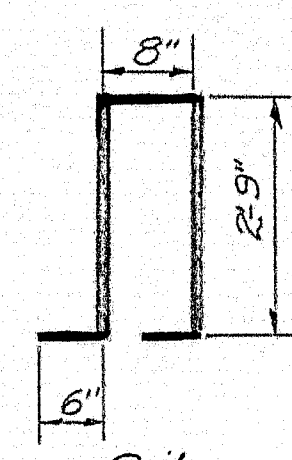
A23



PS 11



F-1



C-1

ABUTMENT #1				
MARK	SIZE	BENT	BARS	LOCATION
		NUMBER	LENGTH	
A 11	# 5	33	5'-10"	BREASTWALL
A 12	# 5	32	7'-2"	BREASTWALL
A 19	# 4	20	7'-2"	WINGS
A 23	# 4	6	4'-2"	END POSTS

ABUTMENT #2				
BENT BARS				
MARK	SIZE	NUMBER	LENGTH	LOCATION
A 19	# 4	36	7'-2"	WINGS
A 23	# 4	6	4'-2"	END POSTS
A 34	# 5	33	10'-3"	BREASTWALL
A 35	# 5	34	9'-0"	BREASTWALL
PS 11	# 4	8	11'-2"	ADDITIONAL STRG. USED IN WIDENING SO. WING FOOTING

DIER #1				
BENT BARS				
MARK	SIZE	NUMBER	LENGTH	LOCATION
PS 1	#4	2	10'-6"	Pier Cap
PS 2		2	11'-0"	
PS 3		2	11'-6"	
PS 4		2	12'-0"	
PS 5		44	12'-5"	
PS 6		38	13'-3"	
PS 7		2	12'-10"	

PIER #2				
BENT BARS				
MARK	SIZE	NUMBER	LENGTH	LOCATION
PS 1	#4	2	10'-6"	Pier Cap
PS 2		2	11'-0"	
PS 3		2	11'-6"	
PS 4		2	12'-0"	
PS 5		44	12'-5"	
PS 6		38	13'-3"	
PS 7		2	12'-10"	

SUPERSTRUCTURE				
BENT BARS				
MARK	SIZE	NUMBER	LENGTH	LOCATION
F 1	#5	149	43'-6"	SLAB
C 1	#4	238	7'-2"	CURBS
STRAIGHT BARS				
F 2	#5	298	42'-8"	SLAB

STRAIGHT BARS				
MARK	SIZE	NUMBER	LENGTH	LOCATION
F 48	#5	4	31'-0"	SLAB
F 49			31'-9"	
F 50			32'-5"	
F 51			33'-1"	
F 52			33'-9"	
F 53			34'-5"	
F 54			35'-1"	

		STRAIGHT		BARS		
A	1	#6	24	25'-0"	FOOTING (Speed)	
A 3	#6	98	5'-0"	"		
A 4	#5	66	3'-0"	FOOTING		
A 5	#	8	7'-6"	BEEHIVEWALL		
A 6	#	8	23'-9"	BEEHIVEWALL		
A 9	#	6	27'-0"	BARKWALL		
A 10	#	6	23'-9"	"		
A-13	#5	33	3'-0"	"		
A-15	#6	31	3'-6"	BACKWALL		
A-16	#6	8	11'-8"	SOUTH WINGS		
A 17	#5	40	2'-4"	BOTH WINGS		
A 18	#	20	7'-6"	SOUTH WING		
A 20	#	8	11'-0"	SOUTH WINGS		
A 21	#5	14	9'-8"	BOTH WINGS		
A 22	#4	4	2'-2"	END POSTS		
A 24	#4	8	9'-9"	BOTH WINGS		
A 25	#6	52	2'-6"	BOTH WINGS		
A 27	#6	8	12'-10"	NORTH WINGS		
A 28	#5	8	12'-0"	" "		
A 29	#5	20	6'-10"	NORTH WING		

		STRAIGHT		BARS	
A 22	#1		4	2'-2"	END POSTS
A 30	#6		32	26'-0"	FOOTING
A 32	#6		100	6'-6"	FOOTING
A 36	#5		33	5'-0"	BACK WALL
A 37			12	27'-4"	BREASTWALL
A 38			12	23'-6"	BREASTWALL
A 39			6	25'-0"	BACKWALL
A 40	#5		6	26'-0"	BACKWALL
A 41	#6		8	20'-11"	FOLTING - SOUTH WING
A 42	#6		6	8'-0"	SOUTH WING
A 43	#6		8	15'-0"	NORTH WINGS
A 44	#5		8	17'-6"	BOTH WINGS
A 45	#4		8	17'-6"	" "
A46	#7		6	6'-0"	NORTH WING
A 47	#5		36	11'-0"	SOUTH WING
A48	#8		8	6'-0"	BREASTWALL
A 49	#5		6	6'-3"	NORTH WING
A 50	#7		14	10'-0"	BOTH WINGS
A 52	#7		7	18'-0"	NORTH WINGS
A 53	#5		36	10'-3"	NORTH WING
A 56			7	19'-11"	SOUTH WING
A 57	#5		8	17'-6"	BOTH WINGS
A 58	#6		8	20'-3"	NORTH WING FOOTING
A 60	#5		7	15'-11"	NORTH WING
A25	#6		86	2'-6"	FOOTINGS - BOTH WINGS
A4	#5		67	3'-0"	FOOTING
A17	#5		81	2'-4"	BOTH WINGS
A15	#6		32	3'-6"	BACKWALL
A61	#5		3	7'-0"	SOUTH WING
A62	#7		7	10'-0"	" "
A63	#7		8	6'-0"	BREASTWALL

PS 8	2	12'-4"	
PS 9	2	11'-10"	
PS 10	2	11'-4"	Pier Cap
PS 11	#4	9'04	Columns
P 1	#8	4	Pier Cap
P 2	#8	4	
P 13	#7	3	
P 14	#7	3	Pier Cap
STRAIGHT BARS			
P 3	#6	8	Pier Cap (Sides)
P 5	#8	10	25'-0" " (Top)
P 6	#9	4	25'-0" " "
P 11	#6	48	5'-6" Footings
D 3	#9	16	12'-8" Dowels (Fib)
D 4	#9	32	13'-8" "
D 5	#9	16	14'-8" "
P 15	#8	64	22'-0" Columns

P 1		2	12'-4"	
P 5 B		2	12'-4"	
P 5 B		2	11'-10"	
P 5 10		2	11'-4"	Pier Cap
P 5 11	#4	65	11'-2"	Columns
P 1	#8	4	31'-6"	Pier Cap
P 2	#8	4	19'-0"	
P 13	#7	3	31'-6"	
P 14	#7	3	19'-0"	Pier Cap
STRAIGHT BARS				
P 3	#6	8	25'-0"	Pier Cap (Cyls)
P 5	#8	10	25'-0"	- (Top)
P 6	#9	4	25'-0"	"
P 11	#6	48	5'-6"	Footings
D 1	#8	64	8'-0"	Dowels (Ftgs)
P 18	#8	16	19'-4"	Columns
P 19	#8	16	18'-4"	"
P 20	#8	16	17'-4"	"
P 21	#8	16	16'-4"	"

F	23	24	25
F 5	336	26'- 0"	
F 4	88	17'- 0"	
F 5	6	1'- 8"	
F 6	4	2'- 4"	
F 7		3'- 0"	
F 8		3'- 8"	
F 9		4'- 4"	
F 10		5'- 1"	
F 11		5'- 9"	
F 12		6'- 5"	
F 13		7'- 1"	
F 14		7'- 10"	
F 15		8'- 6"	
F 16		9'- 2"	
F 17		9'- 10"	
F 18		10'- 6"	
F 19		11'- 2"	
F 20		11'- 11"	
F 21		12'- 7"	
F 22		13'- 3"	
F 23		13'- 11"	
F 24		14'- 7"	
F 25		15'- 4"	
F 26		16'- 0"	
F 27		16'- 8"	
F 28		17'- 4"	
F 29		18'- 1"	
F 30		18'- 9"	
F 31		19'- 5"	
F 32		20'- 1"	
F 33		20'- 9"	
F 34		21'- 5"	
F 35		22'- 2"	

F 54			35'-10"	
F 56			36'-6"	
F 57			37'-2"	
F 58			37'-10"	
F 59			38'-7"	
F 60			39'-3"	
F 61			39'-11"	
F 62	# 5	4	40'-7"	SLAB
C 2	# 4	8	14'-11"	CURB
C 3		4	19'-11"	
C 4			23'-10"	
C 5			23'-3"	
C 6			21'-7"	
C 7		4	6'-11"	
C 8		8	21'-3"	
C 9		8	21'-0"	
C 10		4	6'-8"	
C 11			21'-2"	
C 12			24'-1"	
C 13			8'-9"	
C 14		4	19'-5"	
C 15	# 4	8	15'-3"	CURB
F 63	# 4	270	26'-9"	SLAB
v N.W.T.				
APPROACH SLABS				
A3 1	# 6	310	14'-8"	(Long End slabs)
A3 4	" 4	36	23'-2"	(7' from A"1) spliced
A3 5	" 4	36	25'-8"	(" " A"2) spliced
V.A.B.P				

V.A.B.P.

V.T.H.K.

NOTES

DIMENSIONS ARE TO C OF BARS.
ALL REINFORCING STEEL SHALL BE
INTERMEDIATE GRADE.

Note: As built revisions shown in
black ink. 9-24-64 E. Barnard.

K. N. W. T.

DESIGN- A/R DET- JFM
TRACE- JFM
CHECK- *As Noted*

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

INTERSTATE 95 OVER RIVER ROAD

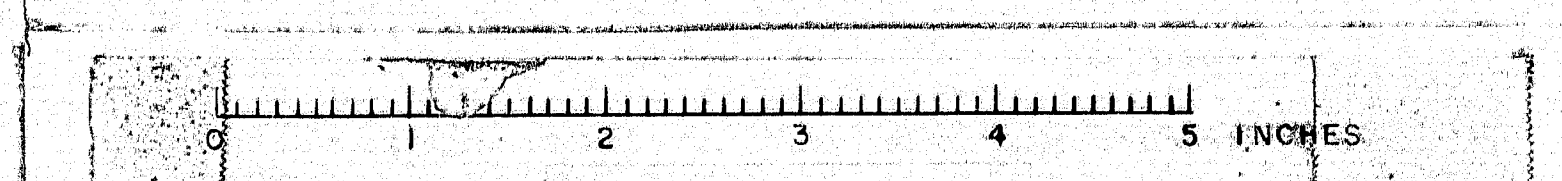
IN THE TOWN OF

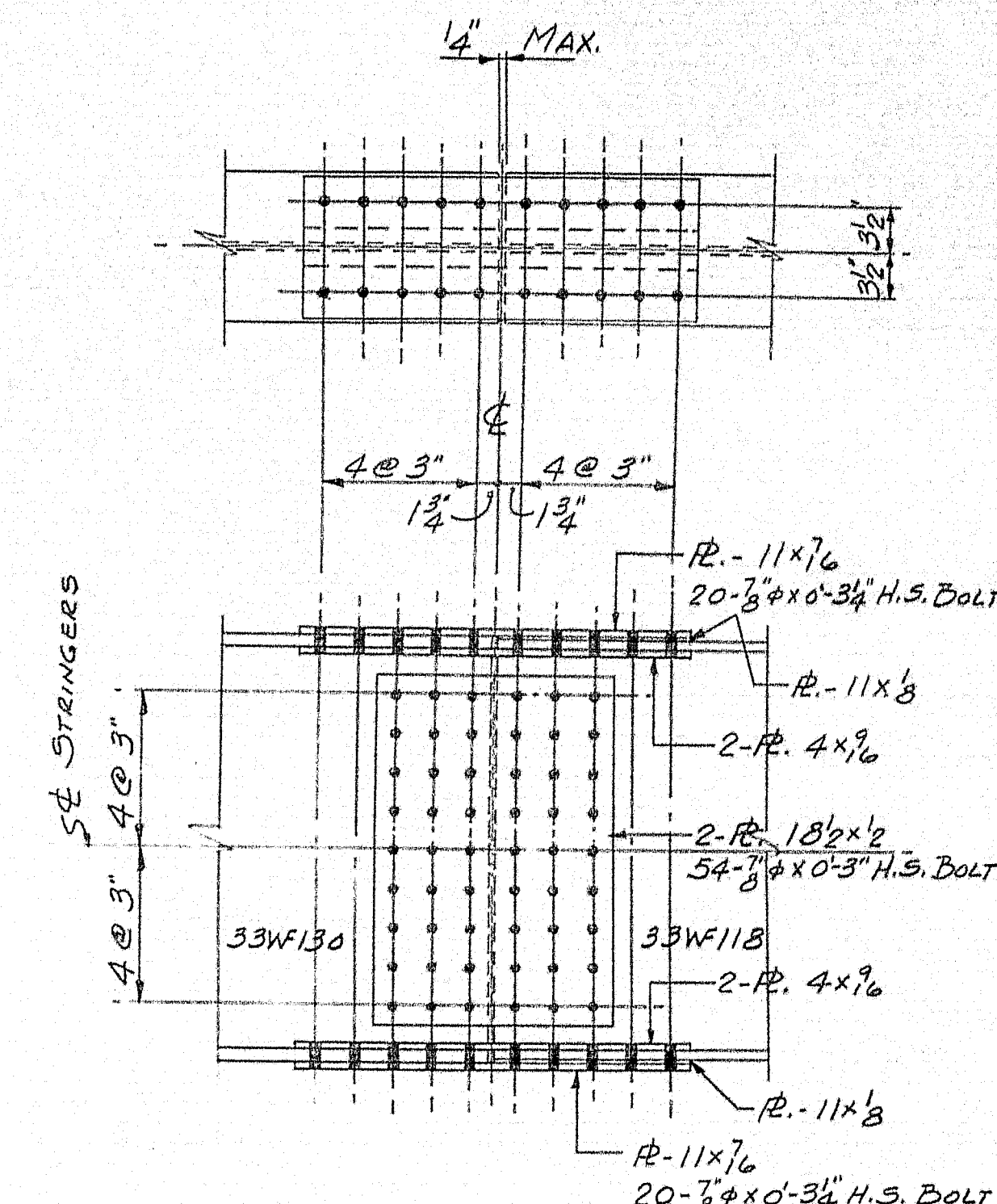
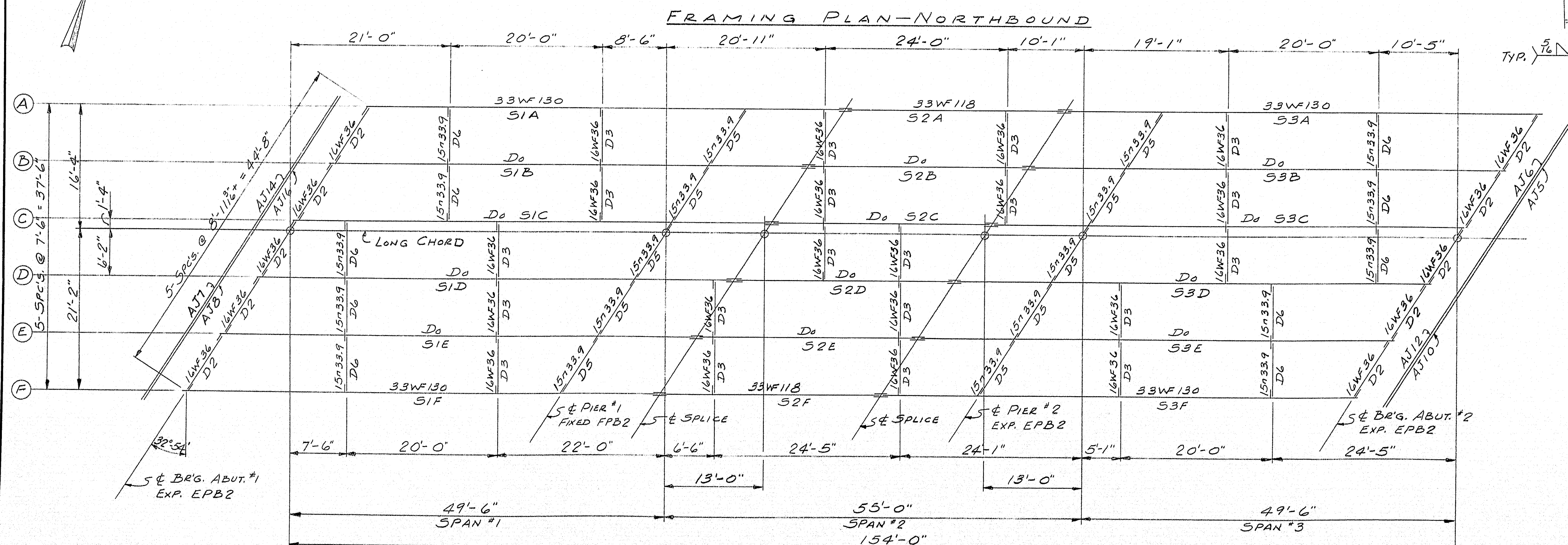
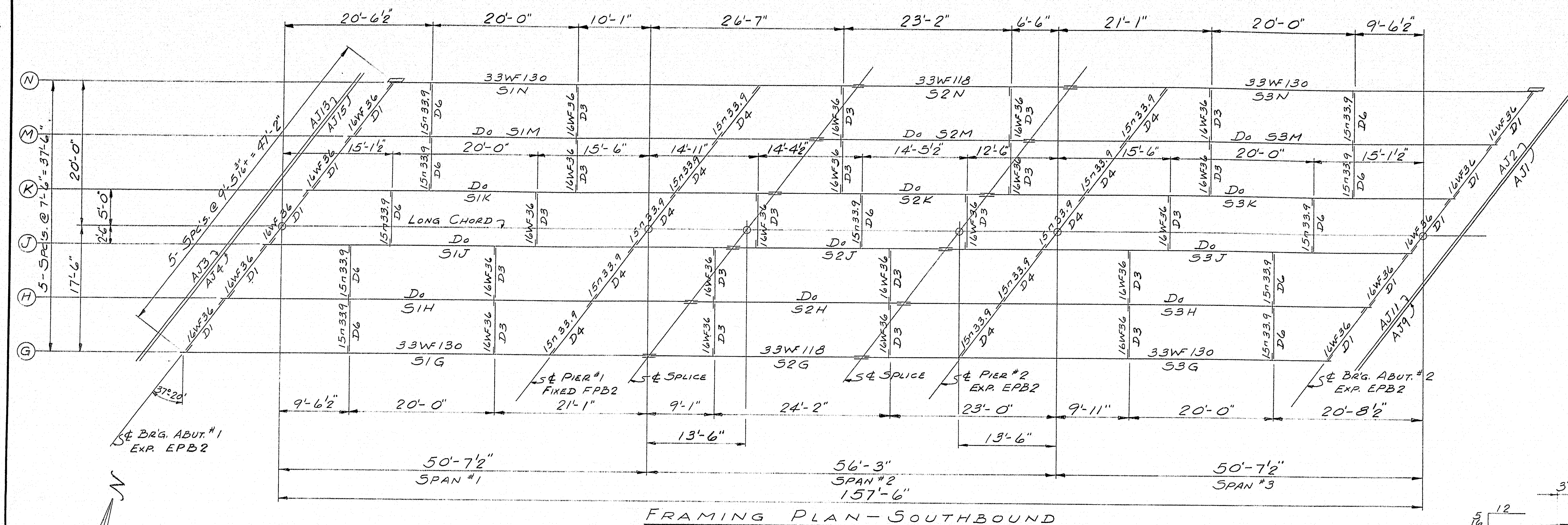
BENTON

KENNEBEC COUNTY

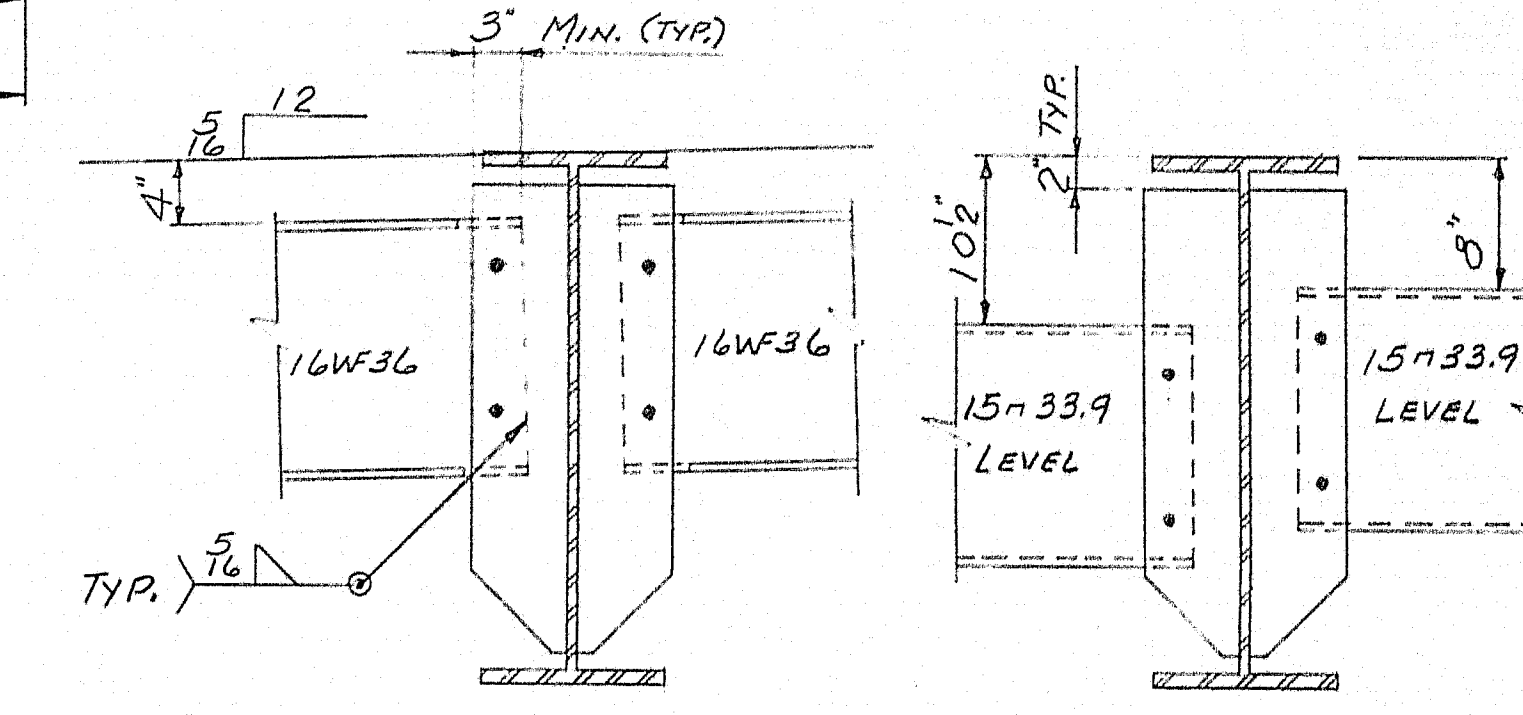
REINFORCING STEEL SCHEDULE NB

SHEET 29 OF 29 AUGUSTA MAINE JUNE 1963





SET STRINGERS TO SLOPE DIAGRAM
DRWG. B63-202-E2, BEFORE DRILLING
& REAMING.

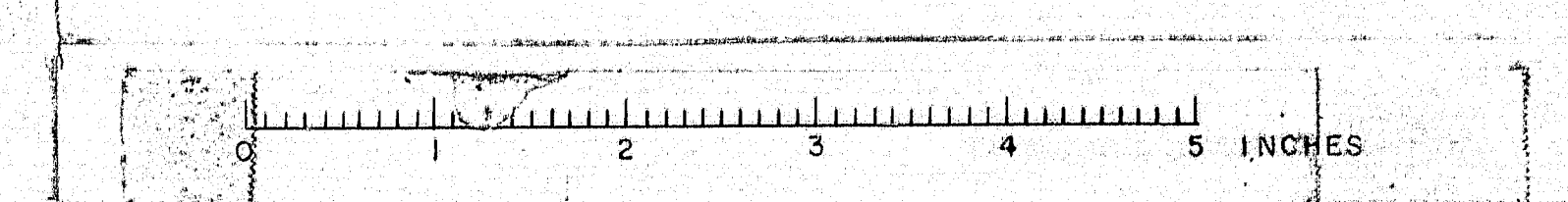


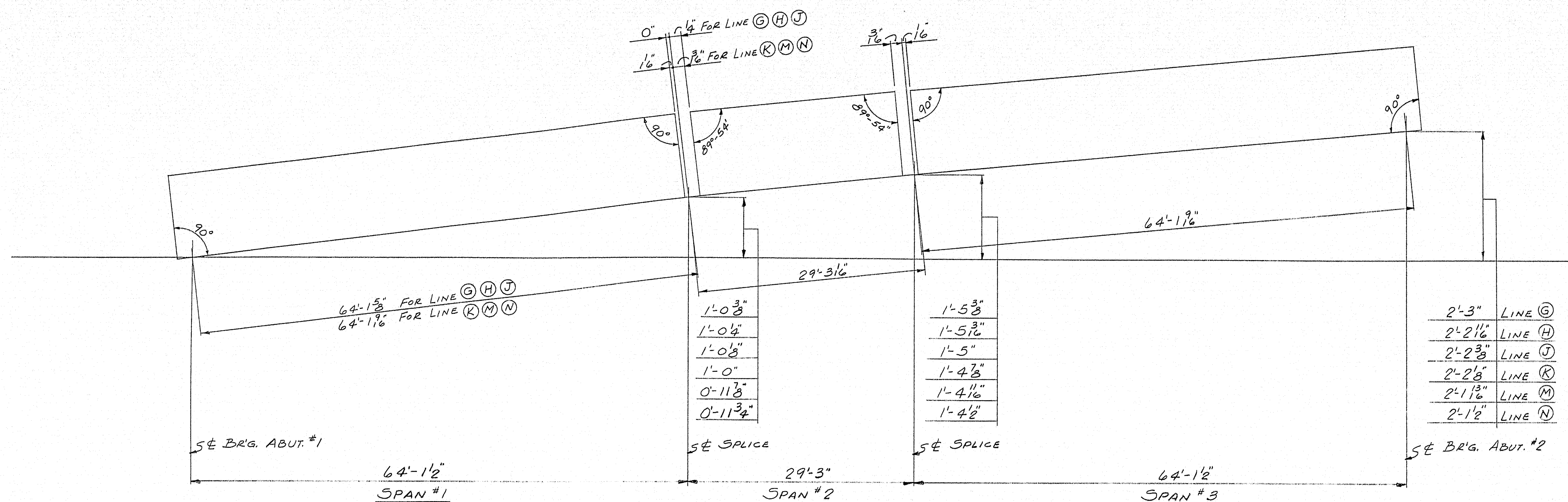
SEE DRWG. #B63-202-E2 FOR GENERAL NOTES

SHOP CONNECTIONS: WELD (E70 ELECTRODE)
FIELD CONNECTIONS: 3/8" H.S. BOLT
HOLES: 1/2" &
PAINT: STATE OF MAINE SPEC'S.

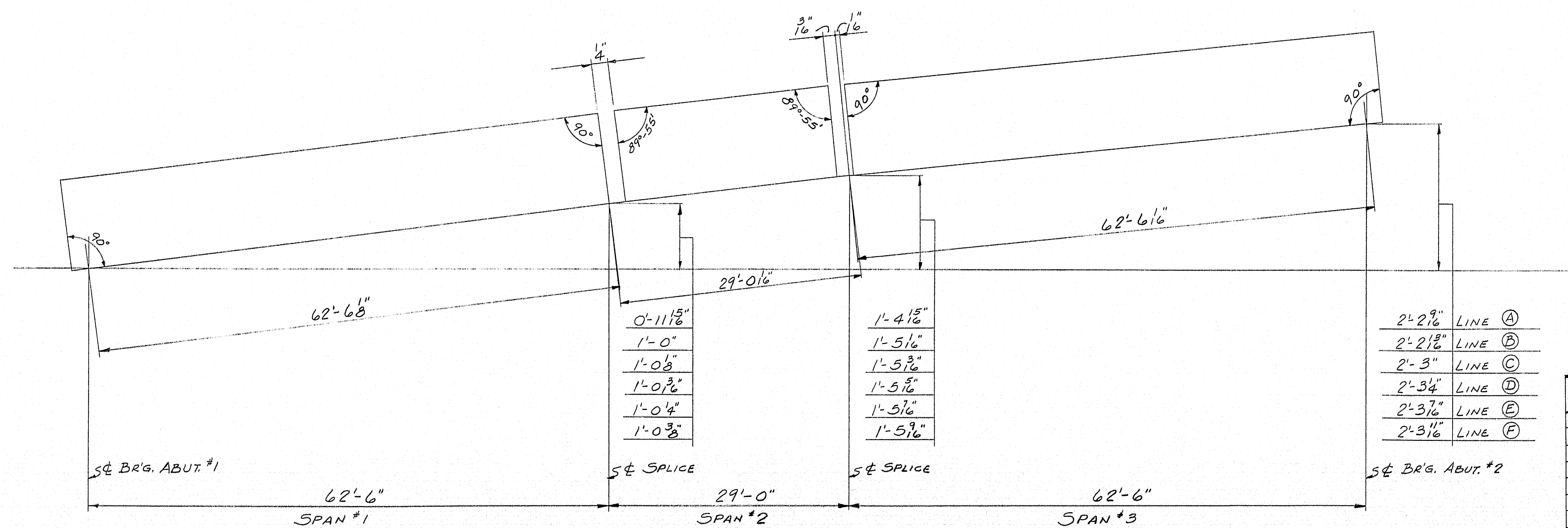
APP'D. 2-12-64

FRAMING PLANS - SOUTHBOUND & NORTHBOUND	
PRINT ISSUE	
3 DIST.	2-13-64
1 SHOP	2-13-64
2 F.A.	1-22-64
DRAWN	12-26-63 R.A.M.
REVISION	
REVISION	
REVISION	
Rancourt & Martin Rolling Mills Company Brewer, Maine INT. #95 OVER RIVER ROAD BENTON, MAINE CUSTOMER: SEAWARD CONST. CO. DESIGNER: STATE HIGHWAY COMM. ORDER: VERBAL DWG: B63-202-E1	





STRINGER DIAGRAM FOR SPLICING - SOUTHBOUND



STRINGER DIAGRAM FOR SPLICING - NORTHBOUND

NOTES

STEEL FOR STRINGERS, & SPLICE RS. SHALL BE ASTM A36. ALL OTHER STEEL TO BE ASTM A36 OR A7.

BOLTS FOR SPLICES SHALL BE ASTM A325, 3/8" HEX HEAVY STRUCT. BOLTS WITH HEAVY SEMI-FINISHED HEX NUTS & ONE HARDENED ROUND WASHER.

HOLES IN STRINGERS ARE FOR H.S. BOLTS. THEY ARE TO BE FREE FROM BURRS. THERE SHALL BE NO PAINT WITHIN 3" OF SUCH OPEN HOLES. SUB PUNCH OR DRILL 1 1/16"; REAM ASSEMBLED (NO TEMPLATES) PARTS TO 1/2" OR DRILL FROM SOLID FOR FIELD CONNECTION. MATCHMARK ALL CONNECTING PARTS BEFORE DISASSEMBLING. SUPPLY ENGINEER WITH A DIAGRAM OF MATCHMARKS.

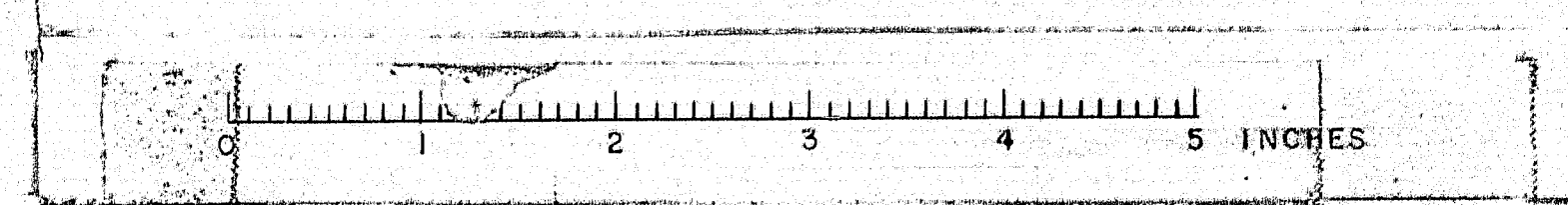
STRINGERS TO BE SET TO GRADE BEFORE DRILLING & REAMING SPLICES.

SHOP CONNECTIONS: WELD (E70 ELECTRODE)
FIELD CONNECTIONS: 3/8" H.S. BOLT
HOLES: 1/2" &
PAINT: STATE OF MAINE SPEC'S.

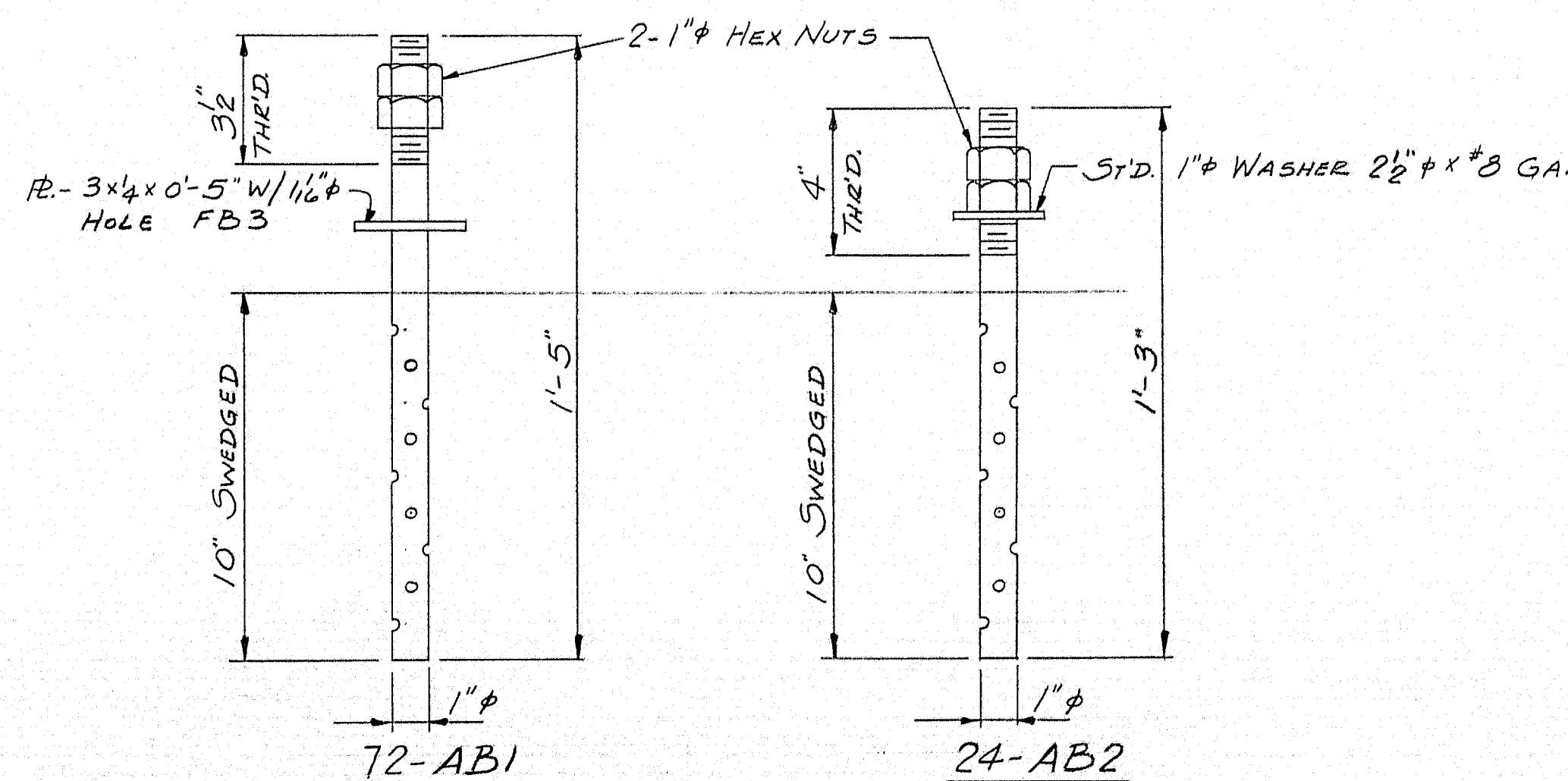
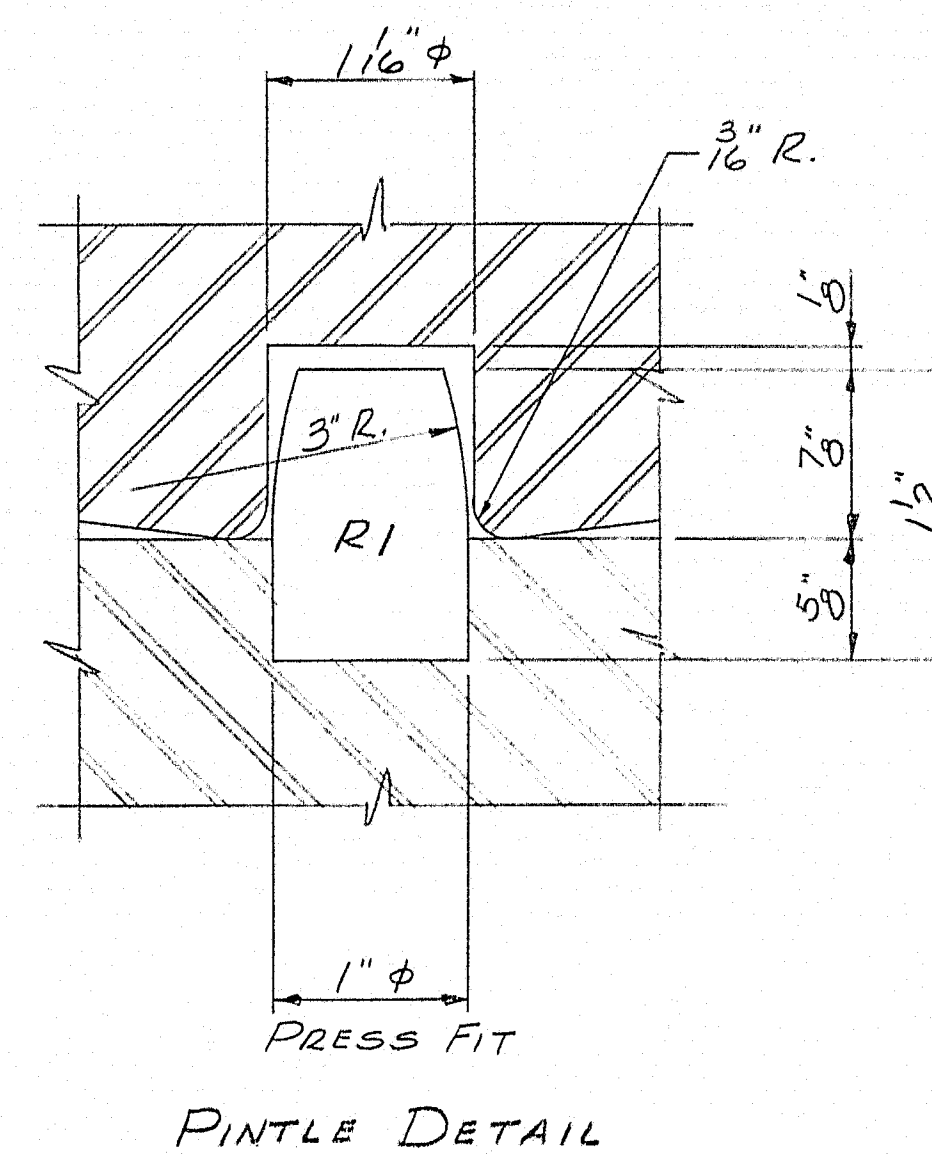
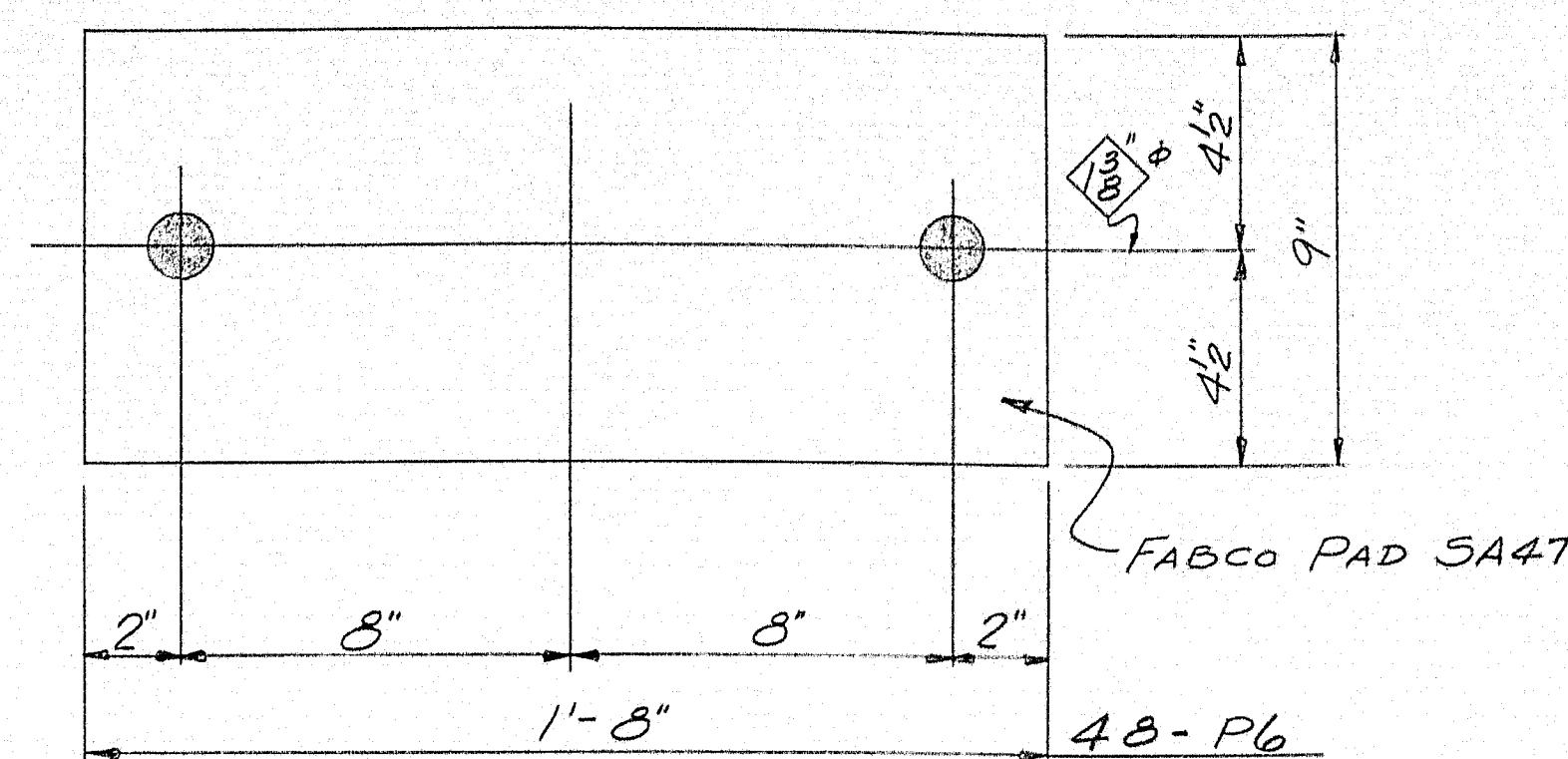
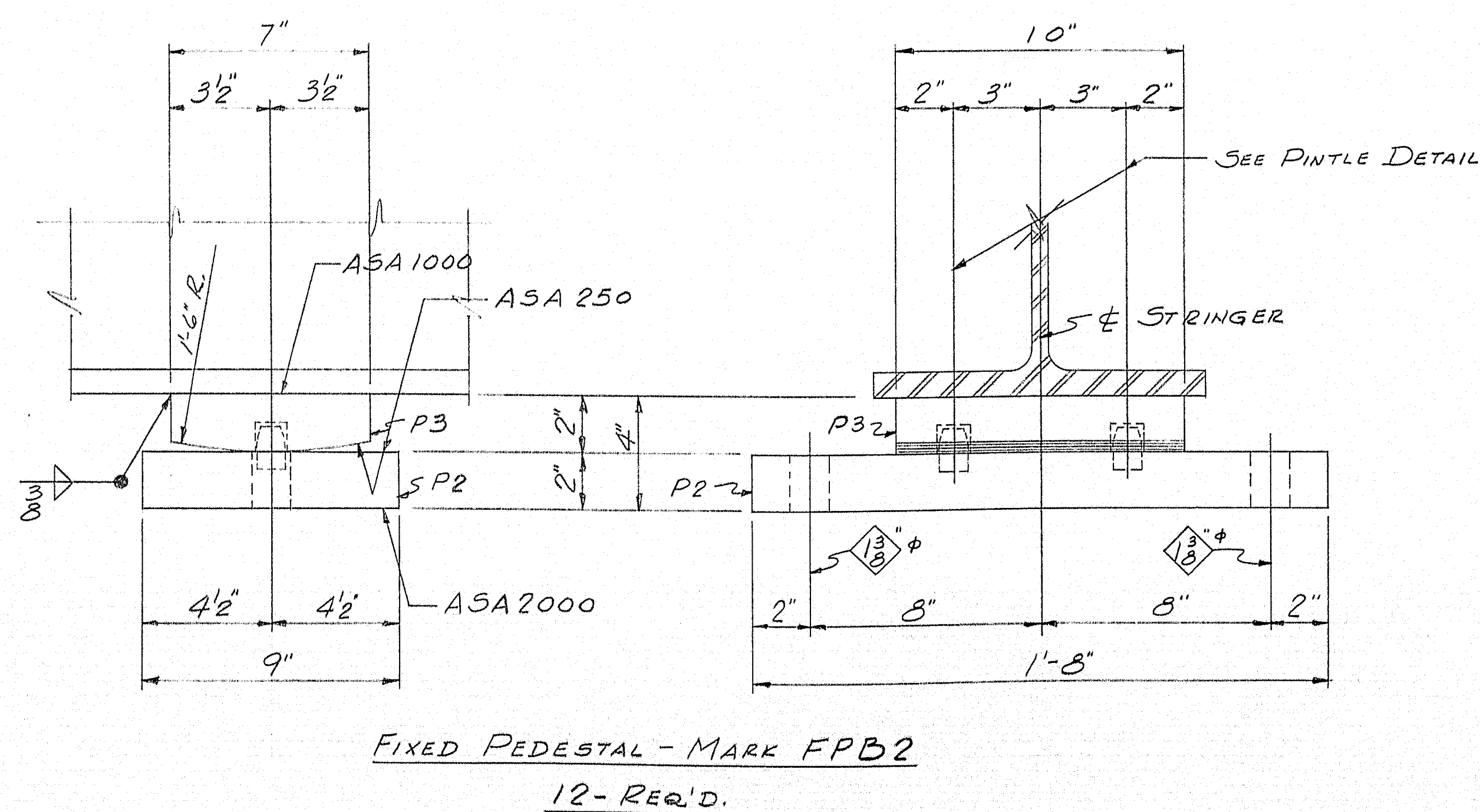
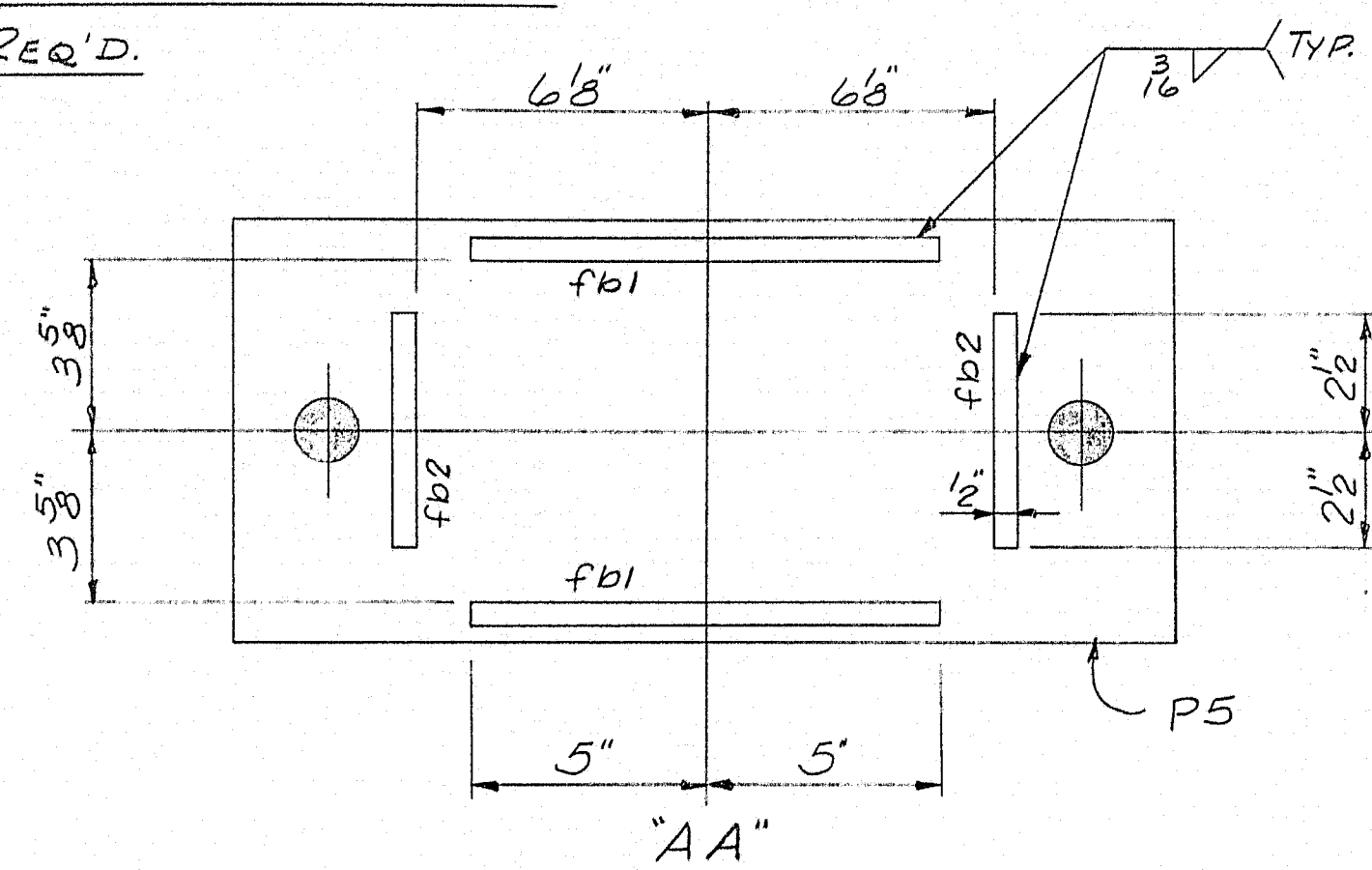
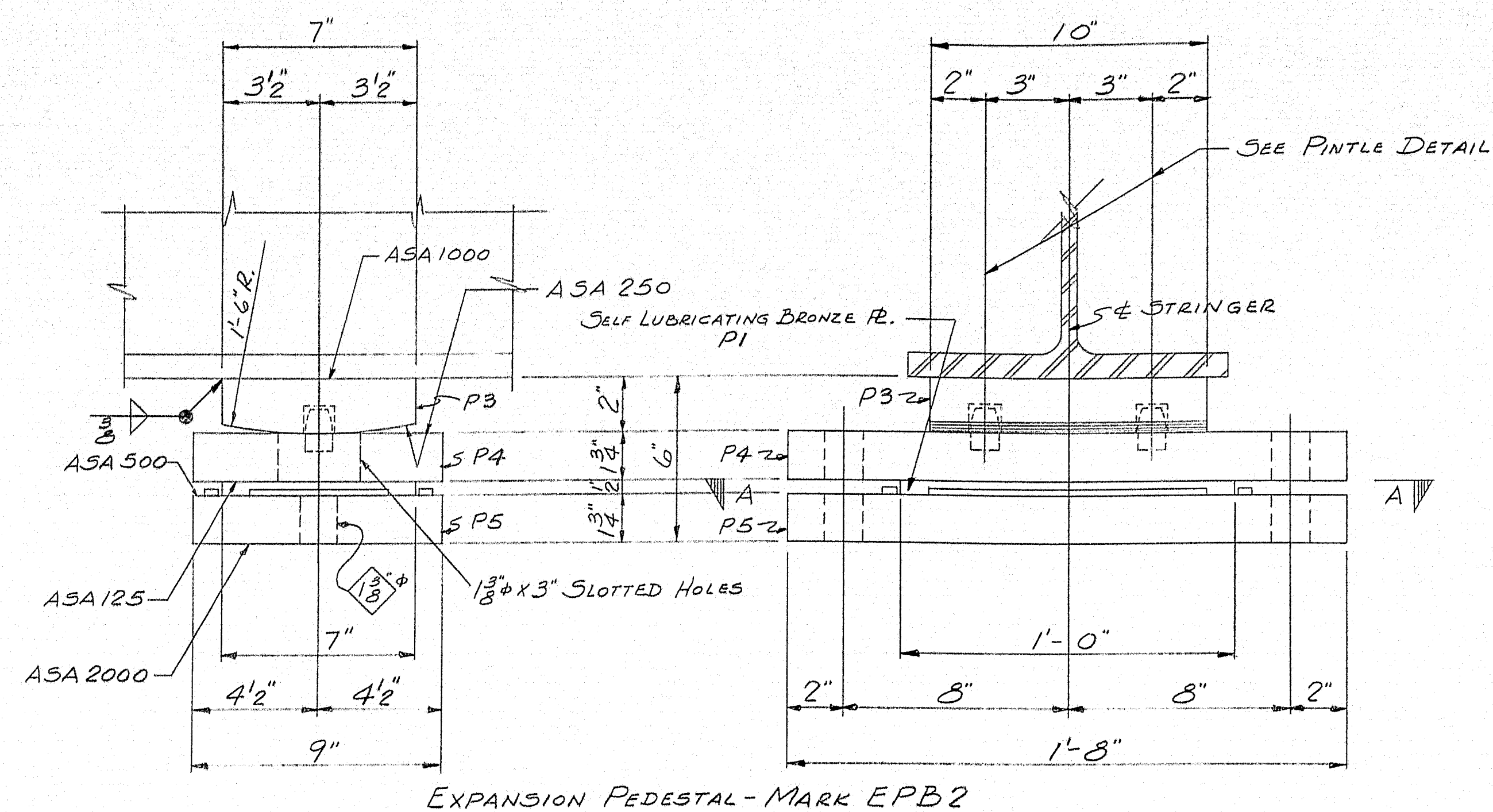
APP'D. 2-12-64

STRINGER DIAGRAMS FOR SOUTHBOUND & NORTHBOUND

PRINT ISSUE			Bancroft & Martin Potting Mill Company Brewer, Maine
NO.	DATE	BY	
3	DIST	2-13-64	INT. #95 OVER RIVER ROAD BENTON, MAINE
2	SHOP	2-13-64	
2	F.A.	1-22-64	CUSTOMER SEAWARD CONST. CO. DESIGNER STATE HIGHWAY COMM.
DRAWN	12-27-63	R.A.M.	
REVISION			ORDER VERBAL DWG. B63-202-E2
REVISION			



90-29-D

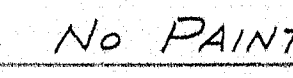
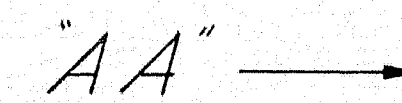
[illegible]

SHOP CONNECTIONS: WELD (E70 ELECTRODE)
FIELD CONNECTIONS: WELD & BOLT
HOLES: AS NOTED
PAINT: STATE OF MAINE SPEC'S.

APP'D. 2-12-64

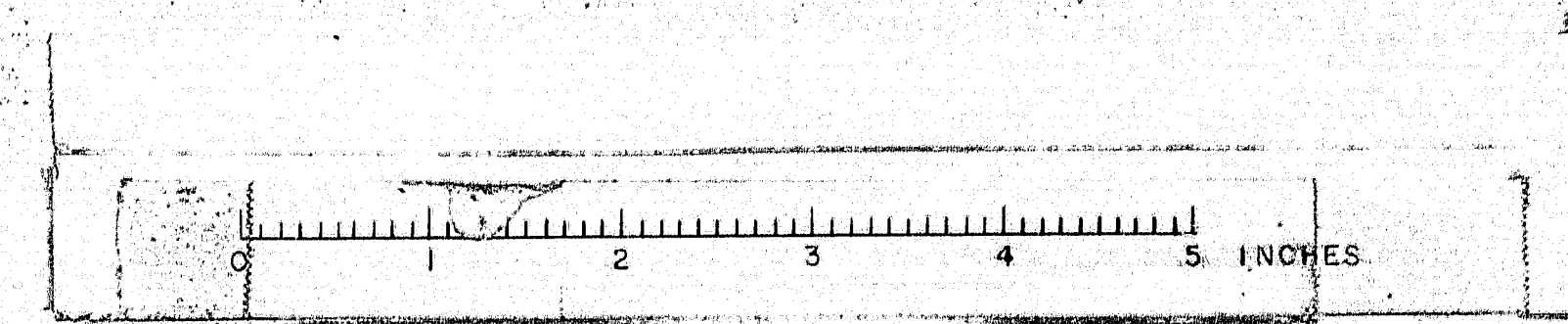
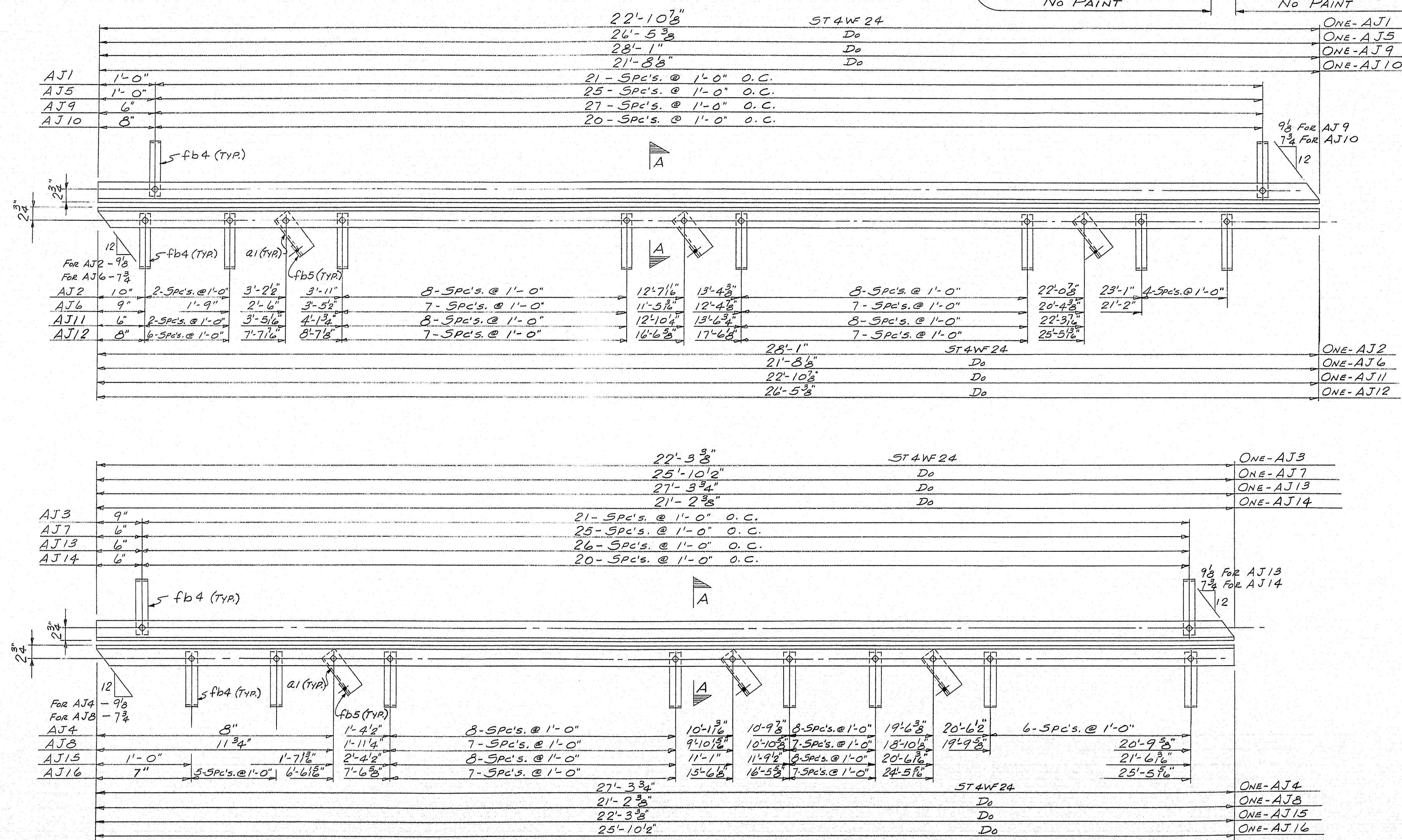
BEARING DETAILS		
PRINT ISSUE		
3	DIST.	2-13-64
4	PORT	1-31-64
2	F.A.	1-22-64
DRAWN 1230-63 R.A.M.		
REVISION		
REVISION		
REVISION		

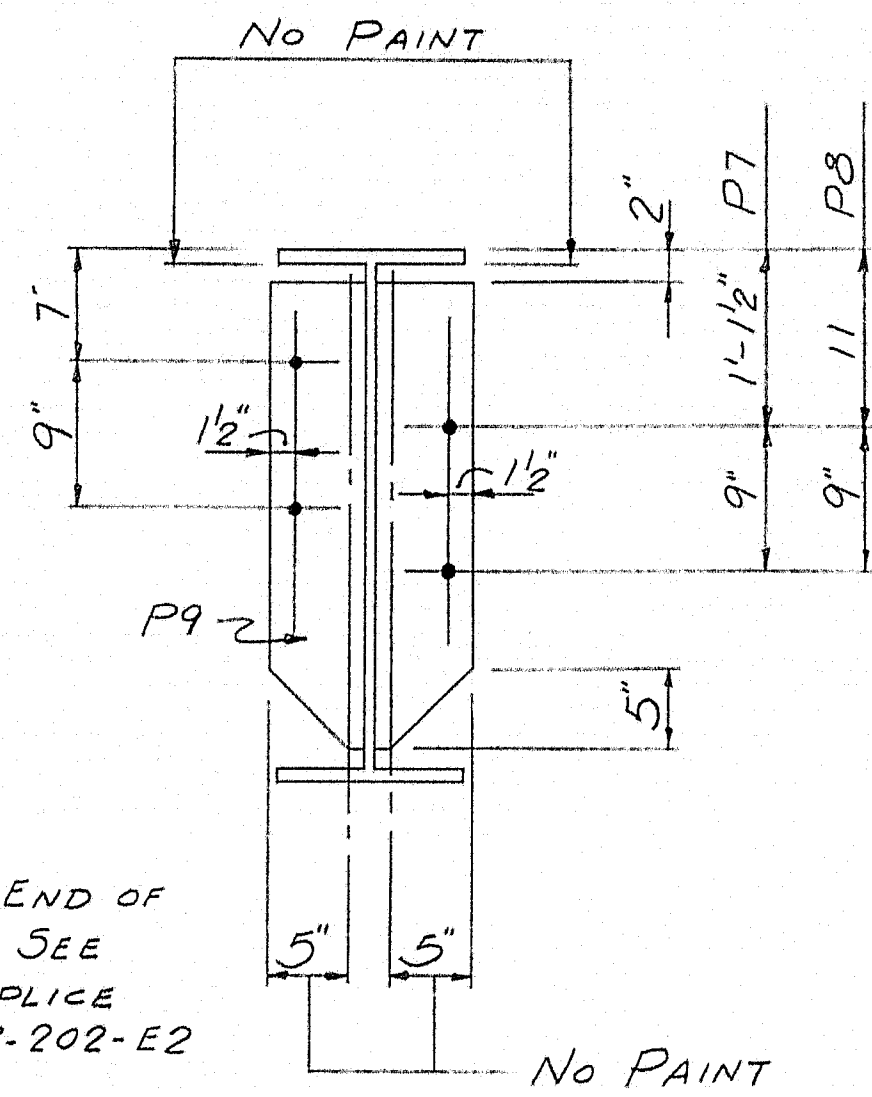
<i>Bancroft & Martin Inc.</i> <i>Brewer, Maine</i>	
INT. #95 OVER RIVER ROAD BENTON, MAINE	
CUSTOMER <u>SEAWARD CONST. CO.</u> DESIGNER <u>STATE HIGHWAY COMM.</u>	
ORDER <u>VERBAL</u>	DWG. <u>B63-202-51</u>



SHOP CONNECTIONS: 7" 8 ϕ H. S. BOLT
FIELD CONNECTIONS: WELD
HOLES: 15" 16 ϕ U. N.
PAINT: STATE OF MAINE SPEC'S.

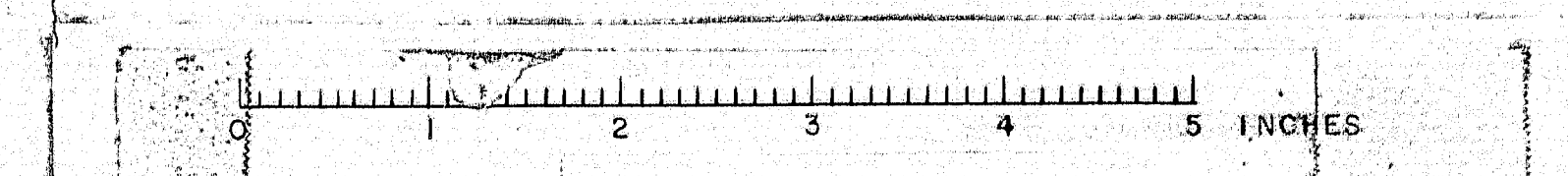
PRINT ISSUE		<i>Bancroft & Martin Inc.</i> <i>Brewer, Maine</i>	
3 DIST.	2-13-64	INT. #95 OVER RIVER ROAD BENTON, MAINE	
5 SHOP	2-6-64		
5 SHOP	2-3-64		
2 F.A.	1-22-64	CUSTOMER <u>SEAWARD CONST. Co.</u> DESIGNER <u>STATE HIGHWAY COMM.</u>	
DRAWN	1-10-64 R.A.M.		
REVISION	2-5-64 R.A.M.		
REVISION		ORDER <u>VERBAL</u>	DWG. <u>B63-202-52</u>
REVISION			

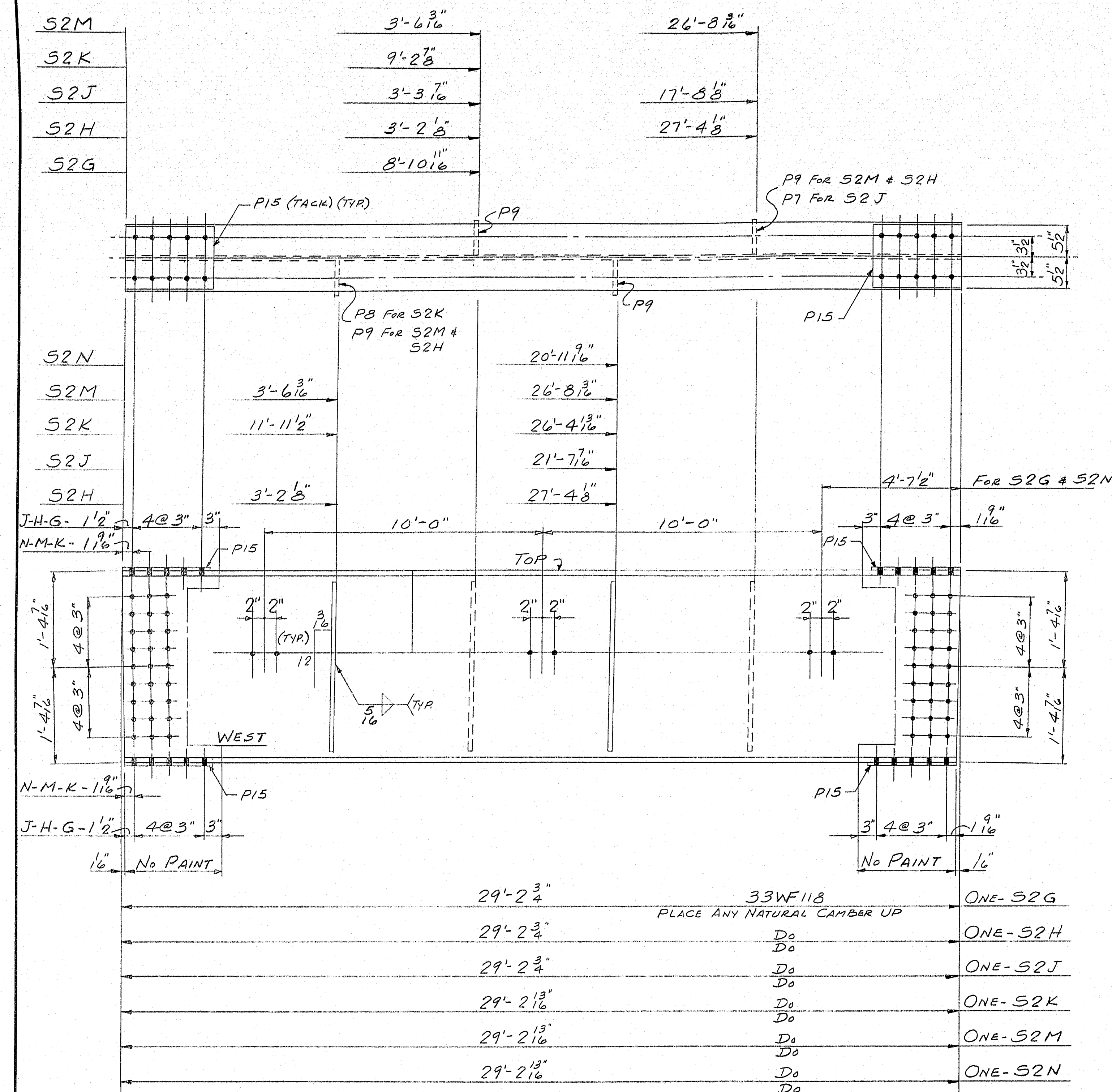




SHOP CONNECTIONS: WELD (E70 ELECTRODE)
FIELD CONNECTIONS: WELD & $\frac{7}{8}$ " H.S. BOLT
HOLES: $\frac{15}{16}$ " ϕ
PAINT: STATE OF MAINE SPEC'S.

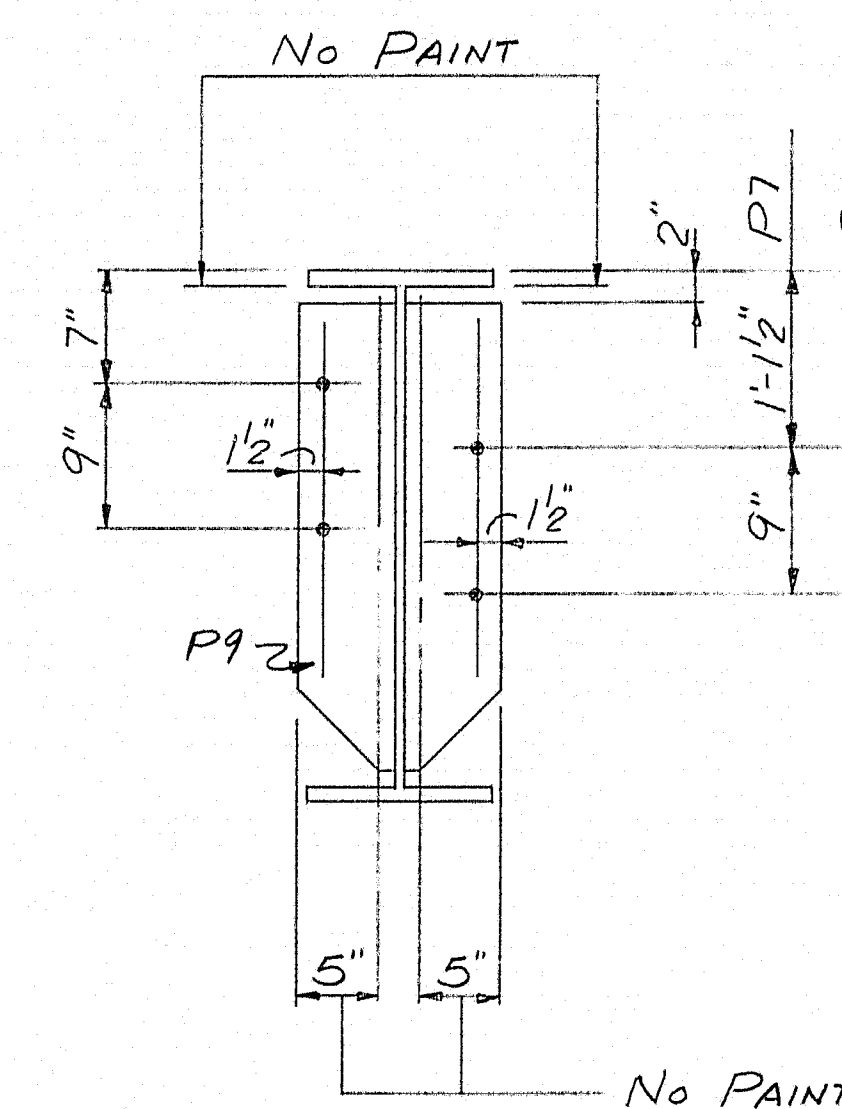
<i>APP'D. AS NOTED 2-12-64</i>			
<i>STRINGERS - SPAN #1 - SOUTHBOUND</i>			
<i>PRINT ISSUE</i>		<i>Bancroft & Martin Inc.</i>	
		<i>Brewer, Maine</i>	
<i>3 DIST. 2-13-64</i>		<i>INT. #95 OVER RIVER ROAD</i>	
<i>5 SHOP 2-13-64</i>		<i>BENTON, MAINE</i>	
<i>2 F.A. 1-22-64</i>		<i>CUSTOMER SEAWARD CONST. Co.</i>	
<i>DRAWN 1-13-64 P.A.M.</i>		<i>DESIGNER STATE HIGHWAY COMM.</i>	
<i>REVISION</i>			
<i>REVISION</i>			
<i>REVISION</i>		<i>ORDER VERBAL</i>	<i>DWG. B63-202-S3</i>





MATCH THIS END WITH EAST END OF STRINGERS S1G THRU S1N. SEE DRWG. #B63-202-E1 FOR SPLICE DRILLING. SEE DRWG. #B63-202-E2 FOR ELEVATION DIAGRAM.

MATCH THIS END WITH WEST END OF STRINGERS S3G THRU S3N. SEE DRWG. #B63-202-E1 FOR SPLICE DRILLING. SEE DRWG. #B63-202-E2 FOR ELEVATION DIAGRAM.

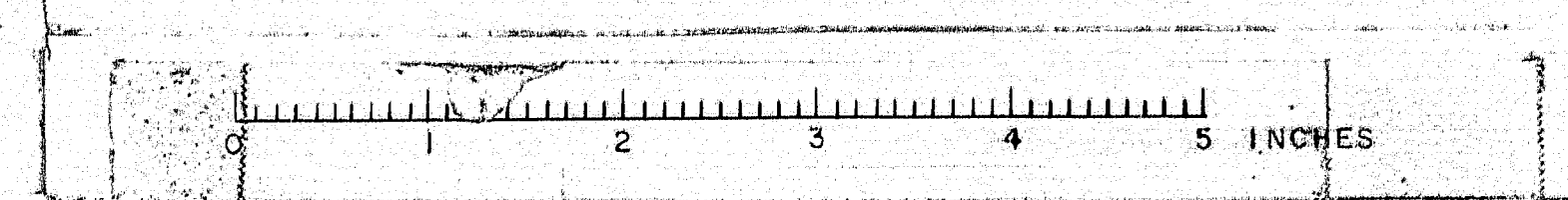


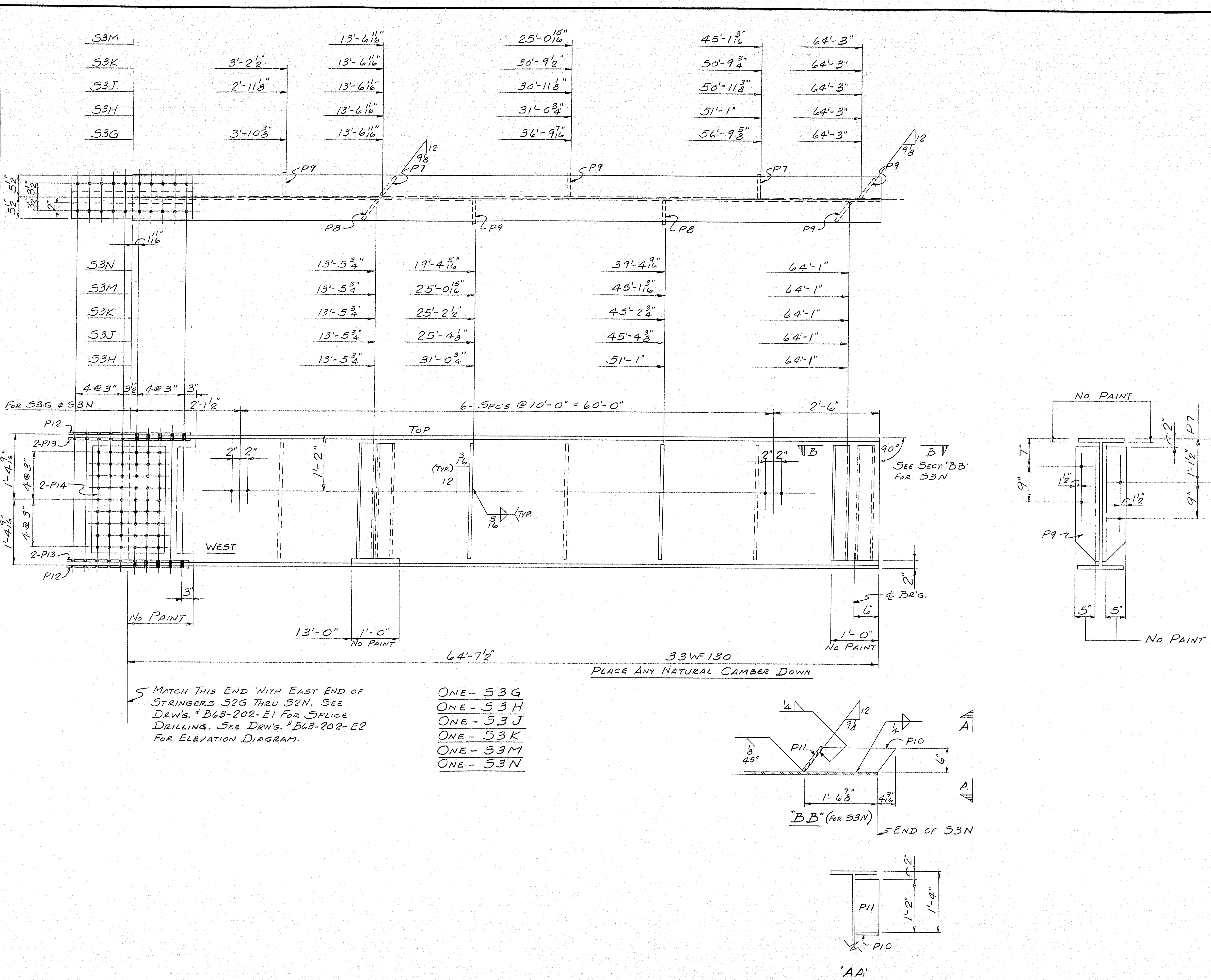
SHIP		BILL OF MATERIAL				DWG. B63-202-54
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
52G	1		33WF118	29' 2 $\frac{3}{4}$ "		A36
52H	1		Do	29' 2 $\frac{3}{4}$ "		
52J	1		Do	29' 2 $\frac{3}{4}$ "		
52K	1		Do	29' 2 $\frac{13}{16}$ "		
52M	1		Do	29' 2 $\frac{13}{16}$ "		
52N	1		Do	29' 2 $\frac{13}{16}$ "		
	1	P7	2. 6x3/8	2 5		A7 or A36
	1	P8	Do	2 5		
	14	P9	Do	2 5		
	24	P15	2. 11x3/8	1 3		A36

SHOP CONNECTIONS: WELD (E70 ELECTRODE)
FIELD CONNECTIONS: WELD #3 & H.S. BOLT
HOLES: 15" &
PAINT: STATE OF MAINE SPEC'S.

APP'D. AS NOTED 2-12-64

STRINGERS - SPAN #2 - SOUTHBOUND			Bancroft & Martin Inc. Brewer, Maine	
PRINT	ISSUE		INT. #95 OVER RIVER ROAD BENTON, MAINE	
3	DIST.	2-13-64	CUSTOMER SEAWARD CONST. CO. DESIGNER STATE HIGHWAY COMM.	
3	SHOP	2-13-64		
2	F.A.	1-22-64	ORDER VERBAL DWG. B63-202-54	
	DRAWN	1-14-64 R.A.M.		
REVISION				
REVISION				
REVISION				

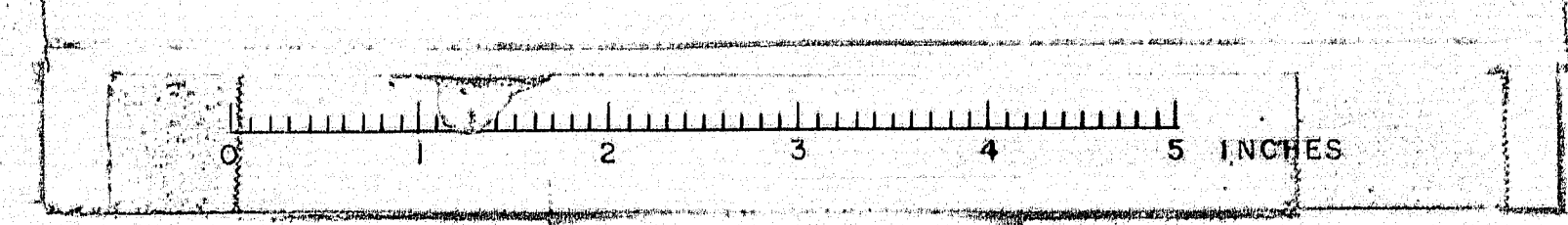


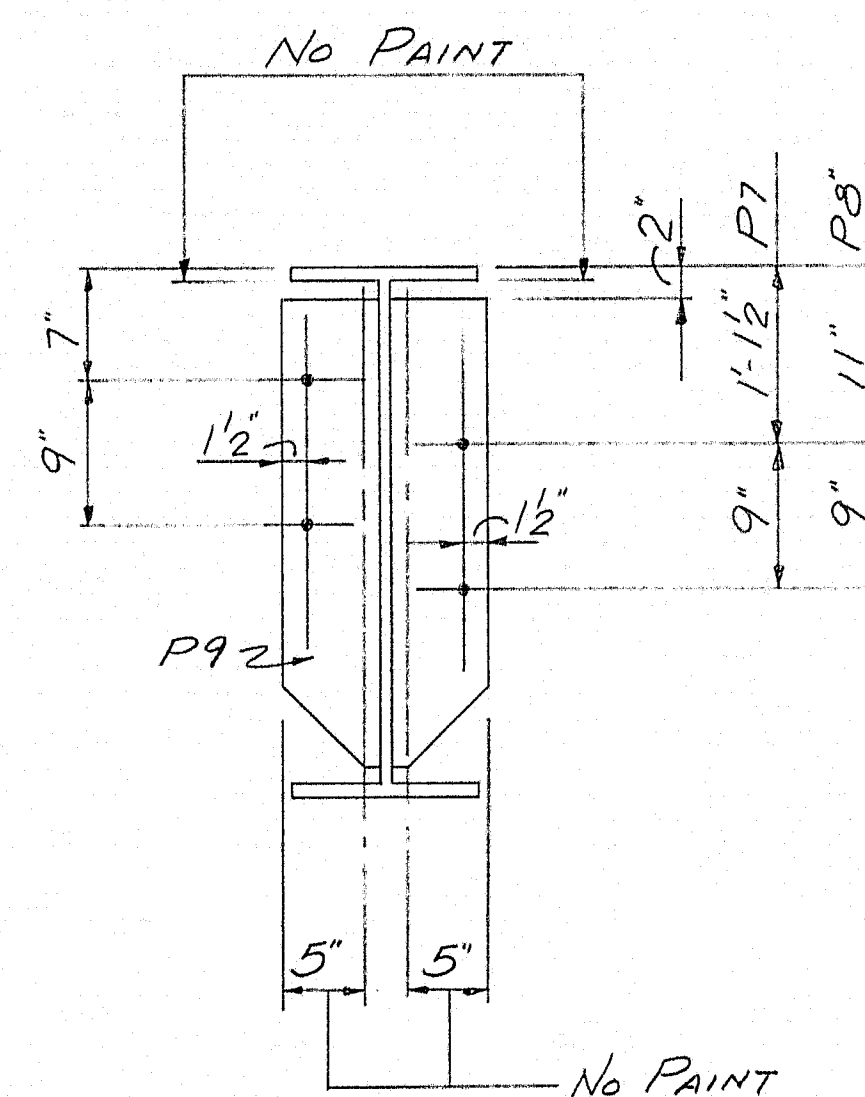
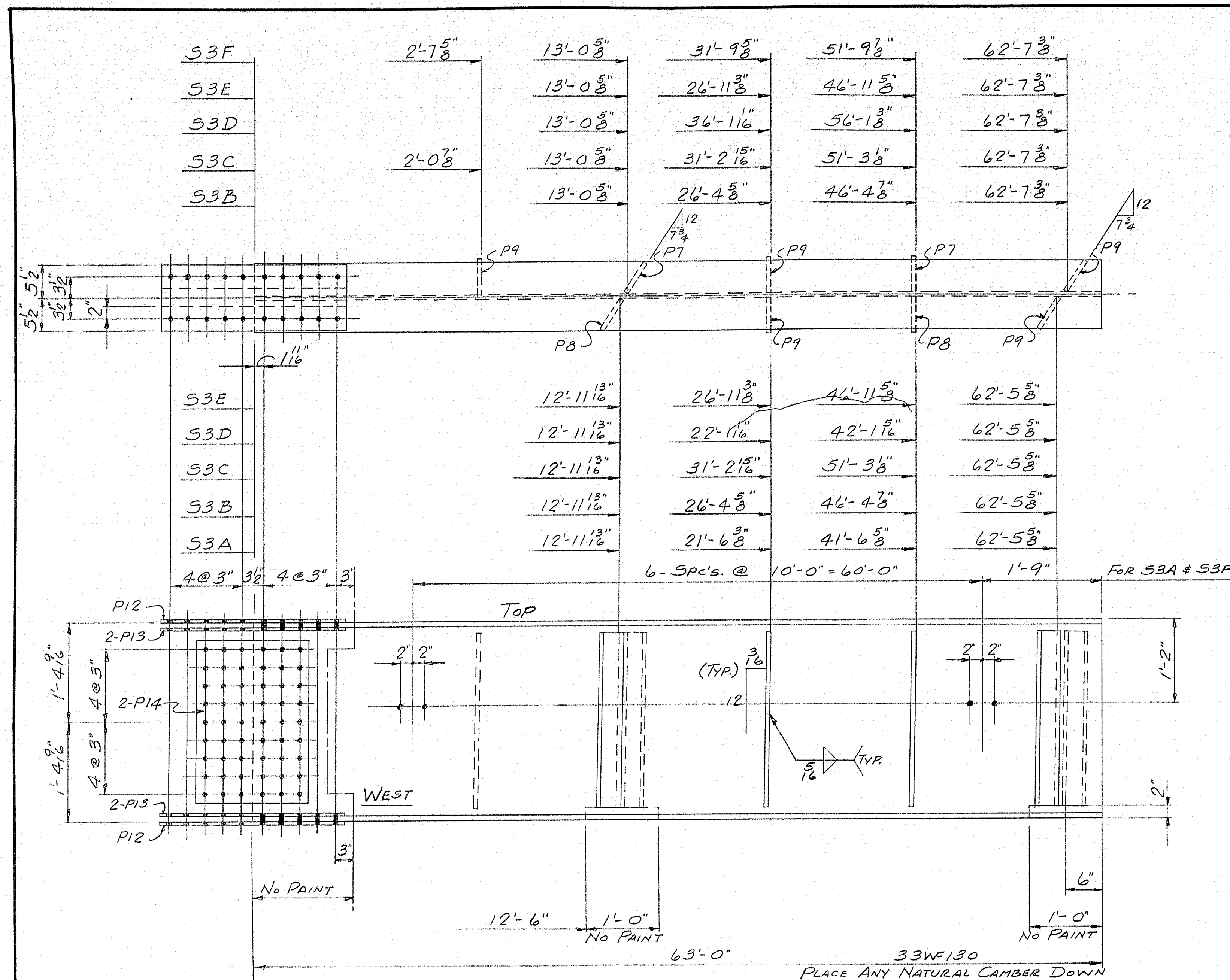


SHIP		BILL OF MATERIAL				DWG. B63-202-55
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
S3G	1		33W130	64' 7 1/2"		A36
S3H	1		D0	64' 7 1/2"		
S3J	1		D0	64' 7 1/2"		
S3K	1		D0	64' 7 1/2"		
S3M	1		D0	64' 7 1/2"		
S3N	1		D0	64' 7 1/2"		
	10	P7	R-6x3/8	2' 5"		A7 or A36
	10	P8	D0	2' 5"		
	23	P9	D0	2' 5"		
	1	P11	R-7 1/2x3/8	1' 2"		
	12	P12	R-11x1 1/8	2' 6 1/2"		A36
	24	P13	R-4x1 1/8	2' 6 1/2"		
	12	P14	R-18 1/2x2	2' 3"		
	1	P10	R-6x3/8	1' 11 1/2"		A7 or A36

SHOP CONNECTIONS: WELD (E70 ELECTRODE)
FIELD CONNECTIONS: WELD # 3" H.S. BOLT
HOLES: 1 1/2" φ
PAINT: STATE OF MAINE SPEC'S.

APP'D. AS NOTED 2-12-64			
STRINGERS - SPAN #3 - SOUTHBOUND			
PRINT ISSUE		Bancroft & Martin Inc. Brewer, Maine	
3	DIST.	2-13-64	INT. #95 OVER RIVER ROAD BENTON, MAINE
5	SHOP	2-13-64	
2	F.A.	1-22-64	
DRAWN	1-15-64	E.A.M.	CUSTOMER SEAWARD CONST. CO.
REVISION			DESIGNER STATE HIGHWAY COMM.
REVISION			ORDER VERBAL
REVISION			DWG. B63-202-55





ONE - S3A
ONE - S3B
ONE - S3C
ONE - S3D
ONE - S3E
ONE - S3F

SHIP		BILL OF MATERIAL				DWG. B63-202-58
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
S3A	1		33WF130	63' 0"		A36
S3B	1		D ₆	63' 0"		
S3C	1		D ₆	63' 0"		
S3D	1		D ₆	63' 0"		
S3E	1		D ₆	63' 0"		
S3F	1		D ₆	63' 0"		
	10	P7	R-6x3/8	2' 5"		A7 or A36
	10	P8	D ₆	2' 5"		
	22	P9	D ₆	2' 5"		
	12	P12	R-11x1/6	2' 6 1/2"		A36
	24	P13	R-4x1/6	2' 6 1/2"		
	12	P14	R-18x2x2	2' 3"		

SHOP CONNECTIONS: WELD (E70 ELECTRODE)
FIELD CONNECTIONS: WELD # 3/8" H.S. BOLT
HOLES: 1 1/2" ϕ
PAINT: STATE OF MAINE SPEC'S.

APP'D. AS NOTED 2-12-64

STRINGERS - SPAN #3 - NORTHBOUND

PRINT ISSUE		Bancroft & Martin Inc. Brewer, Maine	
3	DIST.	2-13-64	INT. #95 OVER RIVER ROAD BENTON, MAINE
5	SHOP	2-13-64	
2	F.A.	1-22-64	CUSTOMER SEAWARD CONST. CO.
DRAWN	1-21-64 R.A.M.		DESIGNER STATE HIGHWAY COMM.
REVISION			ORDER VERBAL
REVISION			DWG. B63-202-58